(ESTABLISHED" 1881.)

NEW SERIES No. 4509.

日七十月二年十三緒光

SATURDAY, APRIL 2, 1904.

六拜禮

\$30 PER ANNUM. SINGLE COPY, 10 CENTS.

Banks.

JOKOHAMA SPECIE BANK, LIMITED. ESTABLISHED 1880. CAPITAL SUBSCRIBEDYen 24,000,000 CAPITAL PAID-UP 18,000,000 CAPITAL UNCALLED...... H

THE

9,320,000 Head Office : -YOKOHAMA. Branches and Agencies.

TOKIO. NAGASAKI. LONDON. NEW YORK, LYONS. HONOLULU. SAN FRANCISCO. SHANGHAL. BOMBAY. NEWCHWANG. TIENTSIN. PEKING.

LONDON BANKERS: THE LONDON JOINT STOCK BANK, LD. PARRS' BANK, LD.
THE UNION OF LONDON AND SMITHS BANK, LD.

HONGKONG BRANCH:-INTEREST ALLOWED. On Current Account at the rate of 2 per cent. per Annum on the Daily Balance. On fixed deposits for 12 months at 5 per cent.

> TARO HODSUMI, Manager.

Hungkong, 11th March, 1904. TONGKONG AND SHANGHAI BANKING CORPORATION.

RESERVE FUND .-Sterling Reserve\$10,000,000 \$16,500,000 RESERVE LIABILITY OF PROPTORS.\$10,000,000

COURT OF DIRECTORS: A. J. RAYMOND, Esq., Chairman. H. E. TOMKINS, Esq., Deputy Chairman. Hon, C. W. Dickson, N. A. Siebs, Esq. E. Goetz, Esq. H. W. Slade, Esq. C. A. Tomes, Esq. A. Haupt, Esq. H. Schubart, Esq. E. S. Whealler, Esq. E. Shellim, Esq.

Hongkong-J. R. M. SMITH. MANAGER: Shanghai-H. M. BEVIS. LONDON BANKERS-LONDON AND COUNTY BANKING COMPANY, LIMITED. HONGKONU-INTEREST ALLOWED: On Current Account at the rate of 2 per Cent.

CHIEF MANAGER:

per Annum on the daily balance. ON FIXED DEPOSITS: For 3 months, 24 per Cent. per Annum. For 6 months, 32 per Cent. per Annum. For 12 months, 4 per Cent. per Annum.

Chief Manager. Hongkong, 20th February, 1904. HONGKONG SAVINGS BANK.

17HE Business of the above Bank is conducted bythe HONGKONG AND SHANGHAI BANKING CORPORATION. Rules may be - obtained on application.

INTEREST on deposits is allowed at 31 PEF CENT, per annum. Depositors may transfer at their optical balances of \$100 or more to the Hongkong AMI

SHANGHAI BANK to be placed on FIXELS DEPOSIT at 4 PER CENT, per annum. For the HONGKONG AND SHANGRAL BANKING CORPORATION.

J. R. M. SMITH, Chief Manager. Hongkong, 151 May, 1902. THE NATIONAL BANK OF CHINA,

LIMITED. Authorised Capital......£1,000,00 HEAD OFFICE:-HONGKONG.

Board of Directors :-Chan Kit Shan, Esq. | J. Focke, Esq. Creasy Ewens, Esq. G. C. Moxon, Esq. Chief Manager, GEO. W. F. PLAYFAIR.

Interest for 12 months Fixed 5 % Hongkong, 4th February, 1904. THE DEUTSCH ASIATISCHE BANK.

PAID-UP CAPITALSh. Taels 5,000,000 HEAD OFFICE-SHANGHAL BOARD OF DIRECTORS: BERLIN.

BRANCHES: Hankow Calcutta Berlin Tsingtau (Kiautschou) Tientsin

LONDON BANKERS: Messrs. N. M. ROTHSCHILD & SONE, Union of London and Smiths Bank, LTD. DEUTSCHE BANK (BERLIN), LONDON AGENCY DIRECTION DER DISCONTO GESELLSCHAFT.

INTEREST allowed on Current Account. DEPOSITS received on terms which may be learned on application. Every description of Banking and Exchange business transacted. H, SUTER, Sub-Manager.

Hongkong, 31st March, 1904.

TO LET.

NO. 1, RIPON TERRACE in FLATS. No. 4, RIPON TERRACE.

Polo Ground.

OFFICES in course of erection, CON-NAUGHT ROAD (near BLAKE PIER). GODOWNS: PRAYA EAST. No. 10, MACDONNELL ROAD.

Apply to THE HONGKONG LAND INVEST MENT & AGENCY CO., LD.

Hongkong, 2nd April, 1904.

INTERNATIONAL BANKING CORPORATION.

CAPITAL, SURPLUS AND UNDIVIDED PROFITS GOLD \$7,992,173.37 = about £1,640,000. CAPITALIAND SURPLUS AUTHORISED GOLD \$10,000,000 = £2,055,000.

HEAD OFFICE I, WALL STREET, NEW YORK. LONDON OFFICE: THREADNEEDLE HOUSE, E.C.

BRANCHES AT SAN FRANCISCO, WASHINGTON,

MEXICO, MANILA, SHANGHAI, SINGA-PORE, YOKOHAMA, BOMBAY, CALCUTTA AND AGENTS ALL OVER THE WORLD LONDON AND CONTINENTAL BANKERS:

NATIONAL PROVINCIAL BANK OF ENGLAND, LIMITED, UNION OF LONDON AND SMITH'S BANK, LTD. CREDIT LYONNAIS, DRESDNER BANK, COMPTOIR NATIONAL D'ESCOMPTE DE PARIS, &C.

THE Corporation transacts every Descrip-I tion of Banking and Exchange Business, receives Money in Current Account and issues Fixed Deposit Receipts either in Gold or Silver at Rates which may be ascertained on Application.

HONGKONG BRANCH: 20, DES VIEUX ROAD CENTAL. CHARLES R. SCOTT,

Manager. Hongkong, 14th December, 1903.

IMPERIAL BANK OF CHINA.

ESTABLISHED BY IMPERIAL DECREE OF THE 12TH NOVEMBER, 1896. Shanghai Taels.

SUBSCRIBED CAPITAL 5,000,000 PAID-UP CAPITAL 2,500,000 Head Office :- SHANGHAL

Brunches and Agencies. PENANG. CANTON. SINGAPORE. CHEFOO. TIENTSIN. HANKOW. PEKING.

I HE Bank purchases and receives for collec-L tion Bills of Exchange drawn on the above places, and Sells Drafts and Telegraphic Transiers Payable at its Branches and Agencies.

HONGKUNG BRANCH. Advances made on approved securities. Bills discounted. INTEREST ALLOWED ON DEPOSITS.

3 2 per Annum Fixed Deposits for 3 months.

Managen Hongkong, 12th August, 1903. THE CHARTERED BANK OF INDIA,

AUSTRALIA AND CHINA. INCORPORATED BY ROYAL CHARTER, 1853. HEAD OFFICE:-LONDON

CAPITAL PAID-UP......£800,000 RESERVE LIABILITY OF SHARE-INTEREST ALLOWED on CURRENT

ACCOUNT at the Rate of 2 per cent. per annum on the Daily Balances. [23] On Fixed Deposits for 12 months, 4 per cent T. P. COCHRANE,

Hongkong, 24th December, 1903.

Hotel.

KING EDWARD HOTEL.

A HIGH CLASS PRIVATE HOTEL

LADIES' AFTERNOON TEA ROOMS.

PRIVATE BAR BLAILIARD ROOMS.

Hot and Cold Water throughout. Electrically Lighted.

Electric Fans (if required). Electric Passenger Elevator to each Floor. Table D' Hote at Separate Tables.

For Terms, &c., apply to the MANAGER.

Hongkong, 23rd October, 1902.

TO LET.

* No. 71, WYNDHAM STREET. CHEAPEST HOUSES IN THE COLONY.

No. 17, Wong Nei Chong Road, facing Race Course.

FLATS in MORETON TERRACE, facing houses and Verandahs. Only \$40 inclusive of Taxes.

WILD DELL BUILDINGS, No. 147, WANCHAI ROAD. Comfortable and Airy Flats of 2 or 3 Rooms, from \$25 inclusive of Taxes. And others to suit various requirements.

S. A. SETH, Land and Estate Broker, Dairy, Farm Co., Ltd. Hongkong, 26th February, 1904.

HONGKONG HOTEL.

Military Band during dinner on Saturday Nights.

Mails.

AND ORIENTAL PENINSULAR

STEAM NAVIGATION COMPANY.

			Ŧ
FOR	STEAMERS.	TO SAIL ON	REMARKS.
YOKOHAMA VIA SHANGHAI,) MOJI and KOBE. (Passing through the Inland Sea).)	FORMOSA	About 5th	Freight and Passage.
LONDON, &c	BALLAARAT	Noon, 9th April	See Special Advertisement.
SHANGHAI	MALTA	About 10th April	Freight and Passage.
LONDON and ANTWERP VIA SINGAPORE, PENANG, COLOMBO, PORT SAID and MARSEILLES	MALACCA	About 13th April	Freight and Passage.

For Further Particulars, apply to

E. A. HEWETT, Superintendent.

Hongkong, 31st March, 1904.

NORDDRUTSCHER LLOYD, BREMEN

STEAM FOR SINGAPORE, PENANG, COLOMBO, ADEN, SUEZ, PORT SAID, NAPLES, GENOA, ANTWERP, BREMEN/HAMBURG; PORTS IN THE LEVANTE, BLACK SEA AND BALTIC PORTS;

ALSO LONDON, NEW YORK, BOSTON, BALTIMORE, NEW ORLEANS, GALVESTON,

AND SOUTH AMERICAN PORTS: Steamers will call at GIBRALTAR and SOUTHAMPTON to land Passengers and Luggage.

N.B.—Cargo can be taken on through Bills of Lading for the Principal Places in Russia.

PROPOSED SAILINGS FROM HONGKONG

(SURJECT TO ALTERATION.)

	(SUB)ECT TO ALTERNATION
STEAMERS.	SAILING DATES.
4 41 4 44 77 77 77	WEDNESDAY, 13th April.
OLDENDORU	WEDNESDAY, 25th May. WEDNESDAY, 8th June
BAYERN	WEDNESDAY, 8th June. WEDNESDAY, 22nd June. WEDNESDAY, 6th July.
SACHSEN	WEDNESDAY, 22nd lune.
ZIETEN	WEDNESDAY, 6th July.
SEYDLITZ	WEDNESDAY, 20th July. WEDNESDAY, 3rd August. WEDNESDAY, 17th August.
- PRINZ REGENT LO	WEDNESHAY and August.
ROO,N	THE STATE OF THE PROPERTY OF T
• !	steamer of the Hamburg-Amerika Linie.

N WEDNESDAY, the 13th day of April, 1904, at Noon, the Steamship "HAMBURG," of the HAMBURG-AMERIKA LINIE, Captain Burmeister, with MAILS, PASSEN-GERS, SPECIE and CARGO, will leave this Port as above, Calling at NAPLES and GENOA. Shipping Orders will be granted till NOON, on MONDAY, the 11th April, Cargo and Specie will be received on Board until 5 P.M., on TUESDAY, the 12th April, and Parcels will be received at the Agency's Office until NOON, on TUESDAY, the 12th April.

Contents of Packages are required. No Parcel Receipts will be signed for less than \$2.50

and Parcels should not exceed Two Feet Cubic in Measurement. The Steamer has splendid Accommodation and carries a Doctor and Stewardesses. Linen can be washed on board.

NORDDEUTSCHER LLOYD.

For further Particulars, apply to .

Hongkong, 30th March, 1904.

MELCHERS & CO., AGENTS.

Untimations.

BRAND

Pure Gream.

See future announcements.

THOMAS' HOTEL

FIRST CLASS HOTEL, most centrally situated; Well furnished and Airy Bedrooms. Monthly Boarders accommodated on very moderate terms. For Particulars apply to

THE MANAGER.

Proprietor.

The round trip from Hongkong to Macao, thence to Canton and back to Hongkong, will be found interesting and enjoyable. WM. FARMER,

Untimations.

Boyril is an ideal food for the strong and the weak. Boyril imparts extra vigor to the healthy, greater strength to the ailing. Bovril is, moreover, a true friend in the kitchen. It adds nourishment, and gives a delightful "twang" to soups, sauces, gravies and entrees.





(MITSUI & Co.)

HEAD OFFICE:-- 1, SURUGA-CHO, TOKYO. LONDON BRANCH: -34, LIME STREET, E.C. HONGKONG BRANCH :- PRINCE'S BUILDINGS, ICE HOUSE STREET.

OTHER OFFICES:

New York, San Francisco, Hamburg, Bombay, Singapore, Sourabaya, Manila, Amoy. Shanghai, Chefoo, Tientsin, Newchwang, Port Arthur, Seoul, Chemulpo, Yokohama, Yokosuka, Nagoya, Osaka, Kobe, Maidzuru, Kure, Shimonoseki, Moji, Wakamatsu, Karatsu, Nagasaki, Kuchinotsu, Sasebo, Miike, Hakodate, Taipeh, &c.

Telegraphic Address: "MILJUI" (A.B.C. and A 1 Codes).

CONTRACTORS OF COAL to the Imperial Japanese Navy and Arsenals and the State Railways; Principal Railway Companies and Industrial Works; Home and Foreign Mail and Freight Steamers.

SOLE PROPRIETORS of the Famous Milke, Tagawa, Yamano and Ida Coal Mines; and SOLE AGENTS for Fujinotana, Hokoku, Hondo, Ichimura, Kanada, Mameda, Mannoura, Onoura, Otsuji, Sasahara, Tsubakuro, Yoshir otani, Yoshio, Yunokibara and other Coals. N. INUZUKA, Manager, Hongkong.

WINE MERCHANTS. 12 QUEEN'S ROAD CENTRAL.

Picnic parties furnished with wines, etc., at a moment's notice.

Contracts made on special terms with Caterers, Committees, Messes and Captains of Steamers. All Wines, Spirits and Beers supplied are guaranteed.

Price list on application.

TELEPHONE No. 135. Hongkong, 23rd July, 1903.

Telephone

No. 75.

AMERICAN WHISKIES.

OLD CREMOENE WHITE RYE. WATERMILL SOUR MASH.

HIGH BALL KENTUCKY RYE. FINE OLD BOURBON (IDES).

O. K. BOURBON.

PURE AMERICAN RYE. MOUNT VERNON RYE.

CALDBECK, MACGRECOR & CO., WINE AND SPIRIT MERCHANTS,

15, Queen's Road.

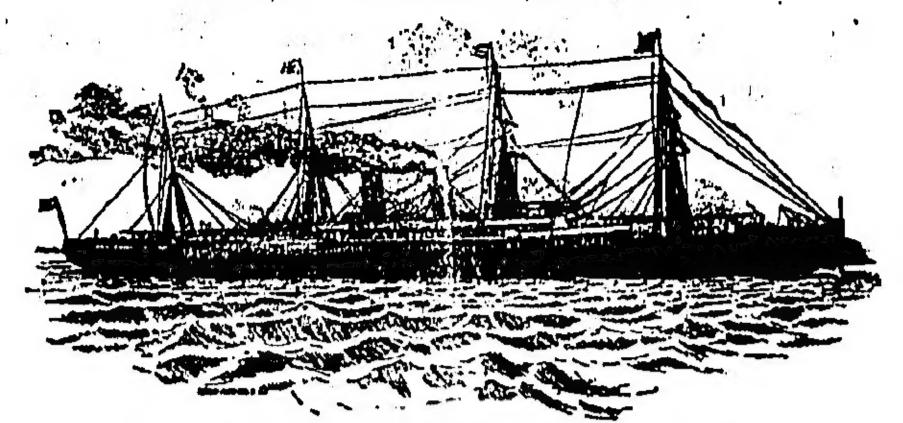
Hongkong, 2nd April, 904.

The attention of consumers is drawn to the fact that the Undersigned, being Sole Agents for DR. AUER VON WELSBACH Co.,

VIENNA, OF INCANDESCENT

GAS LIGHT. ARE SELLING THE ONLY CENUINE MANTLES, The Price of which has been reduced to

FIFTY CENTS per piece. BEWARE OF INFERIOR IMITATIONS



PACIFIC MAIL S.S. CO., OCCIDENTAL & ORIENTAL S.S. CO., TOYO KISEN KAISHA.

TAKING PASSENGERS AND CARGO TO JAPAN, THE UNITED STATES, MEXICO, CENTRAL AND SOUTH AMERICA AND EUROPE:

_		
PROPOSED SAI	LINC	S FROM HONGKONG.
"CHINA" 5,060 Gr	oss To	nsTUESDAY, 5th April, at Noon.
" DORIC " 4.784	91	SATURDAY, 16th April, at Noon.
"SIBERIA"11,284	n	
"COPTIC" 4,352	**	THURSDAY, 12th May, at Noon.
"KOBEA"11,276	11	TUESDAY, 24th May, at Noon,
"GAELIC" 4,205	99	SATURDAY, 4th June, at Noon.
"MONGOLIA" —	21	THURSDAY, 16th June, at Noon.

Record Trip Yokohama to San Francisco made by s.s. "KOREA," 11,276 tons, Oct. 18th-28th, 1902; 10 days, 15 hours.

THE P. M. Company's Steamship "CHINA" will be despatched for SAN FRAN-CISCO, via SHANGHAI, NAGASAKI, KOBE, INLAND SEA, YOKOHAMA and HONOLULU, on TUESDAY, the 5th April, at Noon, taking Freight for Japan, the United States, and Europe. Passengers are allowed to break their journey at any point Through Passage Tickets granted to England, France and Germany by all trans-Atlantic

lines of Steamers, and to the principal cities of the United States or Canada. Passengers holding through ORDERS TO EUROPE have the choice of the Overland Rail Routes from San Francisco, including the SOUTHERN PACIFIC, CENTRAL PACIFIC. UNION PACIFIC, DENVER and RIO GRANDE, and NORTHERN PACIFIC RAIL-WAY; also the CANADIAN PACIFIC RAILWAY on payment of £4 in addition to the

regular tariff rate. Passengers holding Orders for OVERLAND CITIES in the United States have between SAN FRANCISCO and CHICAGO, the option of the SOUTHERN PACIFIC, CENTRAL PACIFIC, UNION PACIFIC, DENVER and RIO GRANDE, and other direct connecting Railways, and from Chicago to destination the choice of direct lines.

Special rates (First-class only) to European Points, are granted to Missionaries, Members of the Naval, Military, Diplomatic and Consular Services, and European Civil Service Officials located in Asia, and to European Officials in the Service of the Governments of China and

TO UNITED STATES and CANADIAN POINTS, Special rates (first class only) are confined and will apply only to Missionaries, Members of the Naval and Military Services, and to Consular and Diplomatic Officials of the Governments of China and Japan.

Through Bills of Lading issued for transportation to Yokohama and other Japan Ports, to San Franscisco, to Atlantic and Inland Cities of the United States, via Overland Railway, to Havana, Trinidad, and Demerara, and to ports in Mexico, Central and South America, by the Companies' and connecting Steamers.

FEATURES OF THIS LINE.

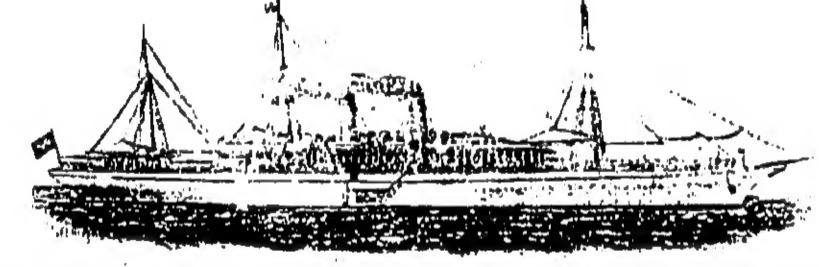
The largest and steadiest and fastest passenger ships on the Pacific. Southern Route; passengers enjoy out-doors throughout; deck bathing. The call at Honolulu. Oahu, the most fertile and beautiful island of the Pacific. The only line to San Francisco, the greatest port of the Pacific.

Sailings positively on schedule date. For further Information as to Passage and Freight, apply to the Agency of the Companies, Queen's Building.

Hongkong, 2nd April. 1904.

E. W. TILDEN, Agent.

CANADIAN PACIFIC RAILWAY COY.'S ROYAL MAIL STEAMSHIP LINE.



THE FAST ROUTE BETWEEN CHINA, JAPAN AND EUROPE, YIA CANADA AND THE UNITED STATES. (Calling at Shanghai, Nagasaki, Kobe, Yokohama & Victoria, B.C.) SAFETY. PUNCTUALITY.

SAVING 3 TO 7 DAYS ACROSS THE PACIFIC. "EMPRESS" Twin Screw Steamships -- 6,000 Tons-10,000 Horse Power-Speed 19 Knots. PROPOSED SAILINGS FROM HONGKONG.—(SUBJECT TO ALTERATION). R.M.S. "EMPRESS OF INDIA" ... 6,000 Tons....... WEDNESDAY, 20th April. "ATHENIAN "...... 3,882 , WEDNESDAY, 27th April. "EMPRESS OF JAPAN"... 6,000 " WEDNESDAY, 11th May. "TARTAR" 4,425 " SATURDAY, 21st May.

Hongkong to London, Intermediate on Steamers, and 1st Class Rail THE magnificent Twin-screw "EMPRESS" Steamships pass through the famous INLAND SEA OF JAPAN, and usually make the voyage YOKOHAMA TO VAN-

COUVER (B.C.) in 12 DAYS, and make connection with the PALATIAL OVERLAND TRAINS FROM THE PACIFIC TO THE ATLANTIC WITHOUT CHANGE. Passengers Booked through to all principal points and AROUND THE WORLD. SPECIAL RATES (First class only) granted to Missionaries, Members of the Naval. Military, Diplomatic and Civil Services, and to European Officials in the Service of Chinese

and Japanese Governments. For further information, Maps, Guides, Hand Books, Rates of Freight and Passage, apply to

Hongkong, 30th March, 1904.

D E BKOWN, General Agent, O, Pedder's Street.

HAMBURG-AMERIKA OSTASIATISCHER FRACHTDAMPFER DIENST.

(Taking Cargo at through Rates to ANTWERP, AMSTERDAM, ROTTERDAM, COPENHAGEN, LISSON, OPORTO, LONDON, LIVERPOOL, GLASGOW, TRIESTE, GENOA, PORTS IN THE LEVANTE; BLACK SEA and BALTIC PORTS; NORTH and SOUTH AMERICAN PORTS).

PROPOSED SAILINGS FROM HONGKONG. CHRIECT TO ALTERATION

	SUBJECT TO ALTERATION		
STRAMERS.	DESTINATIONS.	SAILING DATES	
ABESSINIA	ST. NAZAIRE, HAVRE & HAMBURG. (Calling at SINGAPORE and COLOMBO).	(1 4 1	Freight,
BRISGAVIA	HAMBURG. (Calling at SINGAPORE and COLOMBO).	tath April.	Freight.
yon Döhren	HAVRE and HAMBURG. (Calling at SINGAPORE and PENANG).	25th April.	Freight.
Groumeyer	HAVRE and HAMBURG.	3rd May.	Freight.
MARBURG	HAVRE and HAMBURG. (Calling at SINGAPORE and PENANG).	17th May.	Freight.
Madsen	HAVRE and HAMBURG, (Calling at SINGAPORE and COLOMBO).	} 31st May,	Freight and Passengers
For further F	articulars, apply to	•	_
	HAMBURG-AM	ERIKA LINIR	
And the second	Hongkon	OFFICE.	

No. 1, Queen's Bulldings.

Hongkong, 16th March, 1904.

GO TO THE H

Shipping—Steamers.

HONGKONG, CANTON, MACAO AND WEST RIVER STEAMERS.

JOINT SERVICE OF THE HONGKONG CANTON AND MACAO STEAMBOAT CO., LTD., AND THE CHINA NAVIGATION COMPANY LTD

HONGKONG-CANTON LINE. W. A. Valentine. C. V. Lloyd. Departures from Hongkong to Canton daily at 8 A.M. (Sunday excepted) and at 5.30 P.M.

and 9 P.M. (Saturday excepted). Departures from CANTON to Hongkong daily at 8 A.M., 2.30 P.M. and 5.30 P.M. (Sunday excepted).

These Steamers, carrying His Majesty's Mails, are the largest and fastest on the River Special attention is drawn to their Superior Saloon and Cabin accommodation.

SERVICE OF THE HONGKONG, CANTON AND MACAO STEAMBOAT CO., LTD. HONGKONG-MACAO LINE.

During the summer months the time of leaving fluctuates to suit the tide at Macao. For rther particulars see special time table. Departures on Sundays at 12.30 P.M.

Departures from Macao to Hongkong daily at 7.30 A.M.

CANTON-MACAO LINE.

S.S. "LUNGSHAN,"219 tons,...... Captain T. Hamlin. This steamer leaves Canton for Macao every Tuesday, Thursday and Saturday at about 7-30 A.M.; and leaves Macao for Canton every Monday, Wednesday and Friday at about

JOINT SERVICE OF THE H.K., C. AND MACAO STEAMBOAT CO., LTD., THE CHINA NAVIGA-TION COMPANY, LTD., AND THE INDO-CHINA STEAM NAVIGATION COMPANY, LTD. CANTON-WUCHOW LINE.

"NANNING,"...... ,, C. Butchart. One of the above Steamers leaves Canton for Wuchow every Monday, Wednesday and Friday at about 8 a.m., and the other leaves Wuchow for Canton on the same days at about 8 a.m. Round trips take about 5 days. These vessels have Superior Cabin Accommodation and are lighted throughout by electricity. Further particulars may be obtained at the Office of the-

HONGKONG, CANTON & MACAO STEAMBOAT CO., LD., 18, Bank Buildings, Queen's Road Central, opposite the Hongkong Hotel Or of BUTTERFIELD & SWIRE,

Hongkong, 2nd April, 1904.

ORIENTAL

COSTUMES AND

untimations.

PORTRAITS, GROUPS, ENLARGING, AND COPYING IN ALL SIZES.

> AMATEUR WORK GIVEN SPECIAL ATTENTION.

Agents, CHINA NAVIGATION CO., LTD.

FULL LINE OF SUPPLIES ALWAYS IN STOCK.

FANCY DRAPERIES FURNISHED.

WORK GUARANTEED TO BE THE BEST IN THE COLONY.

SPECIAL TOILET

PATRONAGE RESPECTFULLY SULICITED.

QUEEN'S ROAD CENTRAL.

A FIRST CLASS HOTEL SITUATED NEAR THE BANKS AND PRINCIPAL OFFICES. EXCELLENT CUISINE AND WINES. Large and Lofty Rooms Elegantly Furnished. !lydraulic Elevator. Hot and Cold Water throughout. Special Rates for Tourists. Launch Service for Guests. For Terms, apply to

Hongkong, 1st November, 1902.

THE MANAGER.

EYE-SIGHT.



Mr. N. LAZARUS

May be personally consulted for Spectacles. No charge for testing the eyes.

Glasses and frames of all kinds and qualities. Prices from \$2 upwards.

16, QUEEN'S ROAD, CENTRAL. Hongkong, 6th November, 1901.

F. BLACKHEAD & CO., SHIP-CHANDLERS, SAILMAKERS,

COAL AND PROVISION MER-CHANTS, NAVAL CONTRACTOR: AND GENERAL COMMISSION AGENTS,

16, DES VŒUX ROAD CENTRAL, HONGKONG, SOAF AND SODA MANUFACTURERS.

SOLE AGENTS FOR LI ARTMANN'S RAHTJEN'S GENUINF COMPOSITION RED HAND BRAND, HARTMANN'S GREY PAINT DAIMLER'S PATENT MOTOR LAUNCHES,

Sole Agents for

FERGUSON'S SPECIAL CREAM P. & O. SPECIAL LIQUER SCOTCH WHISKY, &C. BYBRY KIND OF

SHIP'S STORES AND REQUISITES ALWAYS IN STOCK REASONABLE PRICES.

Hongisong, 15th December, 1903.

THE HONGKONG STUDIO,

HIGHER CLASS PHOTOGRAPHER, -41 & 43, QUEEN'S ROAD CENTRAL, TOP FLOOR.

DORTRAITS, GROUPS and ENLAR GING and COPYING in all Sizes.

LARGE SELECTION OF VIEWS ALWAYS ON HAND.

PRICE VERY MODERATE. Hongkong, 15th September, 1903.

MEE CHEUNG,

PHOTOGRAPHER, TOP FLOOR OF ICE HOUSE, IN Ica-House Rout.

T.S now in a position, in his New and Com I, modious Premises, to eclipse, as beretofore, ALL PHOTOGRAPHIC ART PRACTICED in the Colony or in any part of the Far East. GROUPS AND VIEWS

speciality, Gargiong, aand Septembe, 1698. Intimations.

No. 1 DOCK.

Length inside, 514ft. Width of entrance, top 95 ft.; bottom 75 ft. Water on blocks, 27.5 ft. Time to pump out, 4 hours.

No. 2 DOCK.

Length inside, 875 ft. Width of entrance, top 60.5 ft.; bottom 45.8 ft. Water on blocks, 26.5 ft. Time to pump out, 2 hours.

THESE DOCKS are conveniently situated in Yokohama harbour and the attention of Captains and Engineers is respectfully called to the advantages offered for Docking and repairing Vessels and Machinery of every description.

The plant and tools are of recent patterns for dealing quickly and cheaply with work, and a large stock of material is always at hand, (plates and angles all being tested by Lloyds' surveyors).

Two powerful Twin Screw Towboats are available for taking Vessels infor out of Dock, and for taking Sailing Vessels in or out of the bay. The floating derrick is capable of lifting 35 tons.

Steam Launches of Steel or Wood, Lighters, Steel Buildings and Roofs, Bridge Work, and all kinds of Machinery are made on the premises,

Tenders will be made up when required and the workmanship and material will be guaranteed.

The cost of Docking, and repair work, will be found to compare favourably with that of any port in the world.

Telephone: Works, No. 506; General, No. 876. Telegrams, "Dock, Yokohama," Codes A. I. and A. B. C. (4th).

Yokohama, May 11th, 1903.

Hongkong, and July, 1900.

CRAIGIEBURN

PLUNKET'S GAP, the PEAK, near the TRAM TERMINUS, Tel. 56. For Terms, &c., apply to the

MANAGER.

CHINESE IMPERIAL GOVERNMENT 7 PER CENT SILVER LOAN OF 1886, E.

35TH HALF-YEARLY DRAWING.

NTEREST DUE and DRAWN BONDS of this LOAN will be PAYABLE at the Offices of the Corporation on or after the 31st March, 1904.

LIST of DRAWN BONDS can be obtained on application to the Undersigned. For the HONGKONG AND SHANGHAI BANKING CORPORATION,

Agents issuing the Loan, J. R. M. SMITH, Chief Manager. Hongkong, 30th March, 1904.

WANTED.

CHORTHAND CLERKS and TYPE. GREEN ISLAND CEMENT COMPANY, WRITERS, must know the "UP-TO-DATE" system. Address :-

Co The Hongkong Telegraph. Hongkong, 28th March, 1904.

POSITION WANTED.

TOUNG ENGLISHMAN at present in Japan, is desirous of obtaining a position in a Mercantile Firm in Hongkong. Advertiser has had considerable experience in SHIPPING, GENERAL OFFICE WORK, at I P.M., until Wednesday, 6th April.

COMMERCIAL CORRESPONDENCE. Can furnish good references. Apply, in first instance, to

Care this paper. Hongkong, 28th March, 1904.

WINDSOR GARDEN AND RESTAURANT, WONG-NEI-CHONG ROAD, (HAPPY VALLEY),

Past the Race-Course, Under European Management. A TEALS AND REFRESHMENTS VI are served in style at reasonable prices,

Attendance will be found sattifactory. PARTIES' DINNERS, PICNICS, &c., can always be arranged for through the Manager.

The Scenery of and the View from the Garden would be hard to beat, and many customers have called it the loveliest spot in Hongkong. Come once and you will come again.

JACOBS & HUBER, Proprietors. Hongkong, 17th March, 1904.

AMERICAN POTATOES, EX S.S. "CHINA!" "DED RIVERS" AND "BIRBANKS."

FOR SALE By WOO CHEONG, Central Market, Hongkong, 29th March, 1904.

Limited. PORTLAND CEMENT.

GREEN ISLAND CEMENT COMPANY,

In Casks of 375lbs. net 84.75 ex Factory. In Bags of s5ofbs, net \$2.85 ex Factory. SHEWAN, TOMES & CO., General Managers.

Hongkong, 15th August, 1903.

TUBORG BEER.

FIRST Class PILSENER BEER guaranteed free from Salicylic Acid, and any other Chemicals. PRICE \$10.50 per case of 48 bottles (quarts) or 6 dos. pints. Special Prices for Quantities.

Sole Agents :-

GOVERNMENT NOTIFICATION.

TNFORMATION has been received from the Military Authorities that PRAC-TICE with 15-pr. B. L. guns will be carried out from the road near Saiwan Hill in a direction South-East on to Dragon's Back, and with 4.7" Q. F. guns from Lyemun in a direction North-East across the sea on to High Junk l'eak on the 8th April, 1904, between the hours of 10 A.M. and 2 P.M. By Command,

A. M. THOMSON, Acting Colonial Secretary. Colonial Secretary's Office. Hongkong, 30th March, 1904.

THE FIFTEENTH ORDINARY AN. NUAL MEETING of SHAREHOL-DERS in the Company, will be held in the Office of the General Managers, No. 14, Dea Vœux Road, Victoria, on WEDNESDAY. 6th April, at 11.30 A.M., for the purpose of receiving Statement of Accounts and the Report of the General Managers for the year ending-31st December, 1903, declaring a Dividend and electing a Consulting Committee and Auditors. The TRANSFER BOOKS of the Company will be CLOSED from Saturday, 2nd April

SHEWAN, TOMES & Co., General Managers. Hongkong, 19th March, 1904.

THE YANGTSZE INSURANCE ASSOCIA-TION, LIMITED.

NOTICE TO SHAREHOLDERS,

THE FOURTEENTH ORDINARY GENERAL MEETING of the above Association will be held at the Head Office, No. 26, The Bund, Shanghai, on THURSDAY, the 21st April, 1904. at 4.30 o'clock P.M. precirely, for presentation of the Report of the Directors and the Accounts to the 31st December, 1903, the election of Directors and Auditor for the current year, and for the purpose of transacting any other business which may be transacted at an Ordinary General Meeting. The TRANSFER BOOKS of the Association will be CLOSED from the 11th of April

to the 21st of April, 1904, both days inclusive, Members holding proxies for absent Shareiniders must deposit same with the Secretary. for Registration at least Forty-eight hours before the Meeting, ...

By Order of the Board of Directors, W. S. JACKSON, Secretary.

Shanghai, 25th March, 1904. 1447

HERMANOS.

DIAMOND MERCHANTS, TEWEL. LERS AND WATCHMAKERS

EASTMAN'S KODAKS and FILMS, Sole Agents for "OMEGA" WATCHES. OMEGA" is the best, "THREE YEARS" guarantee given to every purchaser.

40. QUERNS ROAD.

"Thatann's Rnighton AL FRESCO FETE, ON BEHALF OF THE POOR ORPHANS AND INVALIDS

OF THE ITALIAN CONVENT To be held in the Compound of the Roman Catholic Cathedral, on SUNDAY EVENING. the 10th April, 1904, from 9 to 11 P.M.

THE Superioress relying on the benevolent tion, for the many proofs she has had of their generosity, hopes to receive and merit a large [54] Honghong, 29th March, 1904.

Houghoug, toth January, 1905.

Mails.

COMPAGNIE DES MESSAGERIES

MARITIMES.

PAQUEBOTS-POSTE FRANCAIS.

NOTICE

STEAM FOR

SAIGON, SINGAPORE, BATAYIA

COLOMBO, PONDICHERRY, CALCUTTA,

BOMBAY, ADEN, DJIBOUTI, EGYPT,

MARSEILLES, MEDITERRANEAN AND

BLACK SEA PORTS, LONDON,

PORTS OF BRAZIL AND RIVER PLATE.

ON TUESDAY, the 5th April, 1904

"ARMAND BEHIC," Captain Flandin, with: Mails, Passengers, Specie and Cargo, will leave

this Port for MARSEILLES, via Ports of Call;

This Steamer connects at COLOMBO with

Cargo and Specie will be registered for Lon-

Shipping Orders will be granted till NOOM.

only on MONDAY, the 4th April, Specie

and Parcels received until 4 P.M. on the same

day. No Cargo will be received on board on

must be left at the Agency's Office. Contents

and Value of Packages are required.

Parcels are not to be sent on board; they

For further Particulars, apply at the Com-

G. DE CHAMPEAUX,

the Australian Line S.S. Oceanies bound for MARSEILLES vid BOMBAY and ADEN.

don as well as for Marseilles, and accepted in

transit through Marseilles for the principal

WITHOUT TRANSHIPMENT.

places of Europe.

TÚESDAY.

pany's Office.

at t P.M., the Company's Steamship

HAVRE, BORDEAUX:

"RAGGING" NURSES.

STORY OF A MOCK TRIAL AND SENTENCE.

A remarkable case of ragging among some

just come to light. Tooting Graveney Common, is a large and buted to biliousness. No doubt a crippled imposing red-brick building, which was only liver with the resulting impure blood, is the recently built by the Metropolitan Asylums cause of more mental gloom than any other Board. It has accommodation for about 700 single thing. And who can reckon up the patients, mile and femile, and it was opened fearful aggregate of pain, loss and fear a year ago.

there are nearly 400 lunatics, there are about can number. You can see these people everythe gates.

three nurses-a head nurse, an assistant, and a cure. Remedies like third nurse. Each has a day off at stated intervals, and her place is taken by a relief

A little time ago a "relie(" came into one of the wards to take the place of one of the been some talk-more or less unfoundedone of the old ladies in the ward.

A MOCK TRIAL. Inquiries were made, and in the course of an hour or so the other nurses on day duty heard the story. Their work over, a council of war was held, and it was decided forthwith to pay the nurse out. One of the girls suggested a "ragging" and a mock trial in one of the bedrooms of the home.

This was adopted with a good dral of glee among some of the younger nurses, and with just a sprinking of spite, perhaps, among some of the others.

The idea was to duck the offender, and as there was no fountain handy it was suggested | 481 that the bathroom at the end of the corridor would answer the purpose. So, when all was quiet, the bath was filled with ice-cold water. and eight nurses marched along in their stockinged feet to the bedroom where the unsuspecting victim lay.

She was fast asleep. Her room was entered silently, and the first intimation she had of what was happening was the sudden jerking away of her bedclothes.

The next moment half-a-dozen pairs of strong young arms were around her. She kicked out hard, and tried to scream, but was held firmly, and one of the girls prevented her

from making much noise. The hapless nurse was carried along the corridor and to the bathroom; but once inside the door she made more violent efforts to defend herself. She knew what her tormentors meant to do-and they told her why.

"PRISONER" GETS PARTLY FREF.

As all the eight young raggers could not crowd into the bath-room, on account of is size, the "prisoner" managed to get one leg and one arm free. "We're going to give you a good ducking-

and serve you right," cried one of the members of the court martial. The prisoner struggled all the harder, and

there was a fierce fight in the bathroom. Two or three times the resisting nurse was carried to the edge of the bath, but every time she wriggled away, exhausted and gazning. In the final Homeric struggle the prisoner's

lip was rather badly cut, and the sight of the blood evidently cooled the ardor of her tormentors, for they censed their efforts to throw her into the bath, and ended by scooping up the water in their hands and throwing it over her. This pretty drama was carried on in the

dark, so that the victim should not be able to identify any of her as-ailants. She recognised the voice of one of them, however, but she was game enough not to "split, when the time came for explanation; and dismissals.

When the "christening" was over the nurse managed to craw' away, with her mouth bleeding, her nightdress torn, and nearly fainting with fatigue.

REFUSED TO MENTION NAMES.

In the morning she went to Dr. Beresford, the medical superintendent, and told him what had happened. She refused, however, to give the names of her persecutors ; but Dr. Beresford had his suspicions, and sent for two nurses, whom he taxed with the ragging.

They adm tted that they had taken part in it, and the superintendent at once discharged them. They-like their victim-absolutely refused to mention names.

Upon this the six remaining nurses came forward and admitted complicity. They said if the other two girls had gone they would go. And they went.

Seen by a press representative Dr. Beresford, the medical superintendent, described the whole thing as a foolish, an unseemly, and an unfortunate occurrence. "It was absolutely necessary to dismiss the guilty nurses," he said

" and that was done without the slightest delay." The representative was informed that the nurse who was " ragged" was herself dismissed several days later. She was out on night leave and instead of returning by the gates, and reporting herselt in the usual manner, it is stated that she entered the Nurses' "ome by crawling

through one of the windows. "If the nurse has been dismisted, all i can say is that I have not heard of it," was Dr. TRADE Beresford's only remark with regard to this

ANOTHER VERSION OF THE CASE.

The current number of the Hospital gives a version of the incident. One of the dismissed rurses wrote to that journal, and gaue her view of the case. Whereupon the Hospital made inquiries, and reports as

"We are able to give the official statement of the case, which differs considerably from the rumors which had obtained currency. It appears that one day the week before list a junior nurse preferred a request to the medical superintendent, in the presence of the matron, that she might be transferred to another ward giving as the reason that the charge and deputy-charge nurses in her ward were making things too hot for her.'

"Accordingly, the medical superintendent sent for the two nurses, and, in the presence of the matron, told them that any attempt to bully their juniors would be severely dealt with. Forty-eight hours afterwards eight of the nurses repaired to the bedroom of the junior, dragged her out of bed, carried her along to the bathroom, which was close at hand, and endeavoured to give her a cold ducking. The girl resisted so effectually that she was not immersed, only much sprinkled with water, and her lip set in the fray."-- Ru.

Untimations.

THE BRIGHT SIDE

of life. It is a feeling common to the majority of us that we do not get quite the amount of happiness we are entitled to. Among the countof the nurses at the Toot ny Bec Asylum has less things which tend to make us more or less miserable ill health takes first place. Hannah The asylum, which stands at one corner of More said that sin was generally to be attriarising from the many ailments and diseases Most of the patients are between 50 and 70 | which are familiar to mankind; like a vast years of age. In the female portion, in which cloud it hangs over a multitude no one 70 nurses, who live in the ' home " just inside | where. For them life can scarcely be said to have any "bright side" at all. Hence the In each ward in the asylum proper there are eagerness with which they search for relief and

WAMPOLE'S PREPARATION have not attained their high position in the confidence of the people by hald assertions and boasting advertisements. They are obliged to win it by doing actually what, is three nurses. At about this time there had claimed for them. That this remedy deserves its reputation is conceded. It is palatable as about the treatment of some of the patients, honey and contains the nutritive and curative and on the afternoon of the "reliefs" appear- properties of Pure Cod Liver Oil, combined ance it is alleged that she went to the matron with the Compound Syrup of Hypophosphites and reported that the nurse whose place she and the Extracts of Malt and Wild Cherry. was temporarily taking had been ill-treating Nothing has such a record of success in Scrofula, emaciating complaints and disorders, that tend to undermine the foundations of strength and vigour. Its use helps to show life's brighter side. Dr. H. L. Reddy, B A. M. D., L. R. C. S., Edinburg -L R. C. P., London, -Physician Woman's Hospital-Professor University of Bishops College, Canada, says ; " I have much pleasure in stating that I have used it in cases of debility and have found it to be a very valuable remedy as well as pleasing to take." You can take it with the assurance of getting well. It never disappoints, Sold by all

BANK HOLIDAY.

TN accordance with the provisions of Ordinance No. 6 of 1875 the EXCHANGE BANKS will be CLOSED for the Transaction of Public Business on EASTER MONDAY, the 4th instant.

Hongkong and April, 1901 FIRE INSURANCE ASSOCIATION OF HONGKONG.

EASTER HOLIDAY.

TT is hereby notified that FIRE INSUR-ANCE OFFICES will be 'LOSED for the Transaction of Public Business on MONDAY, the 4th instant.

By Order, A. R. LOWE, Secretary. Hongkong, and April, 1904.

VICTORIA RECREATION CLUB.

SEVENTH ATHLETIC MEETING.

RECREATION CLUB request the so to make it. pleasure of the Company of the LADIES of Hongkong on EASTER MONDAY, the 4th April, at I P.M., on the Hongkong Football Club Ground (kindly placed at the disposal of the Committee) at the Happy Valley. By kind permission of Lt.-Col. W. S. BIRD-

WOOD and Officers, the Band of the 110th Mahratta Light Infantry will perform during the afternoon.

Admission to the Grand Stand and Enclosure (Gentlemen) \$1.00. Members of the Hongkong Football Club

on presenting Membership Ticket. Free. No Persons other than Officials and Competitors allowed within the Course, HAROLD C. AUSTEN,

Actg. Hon. Secretary, V. R. C. Hongkong, 31st March, 1904.

THE AMERICAN SYSTEM

DENTISTRY.

DR. M. H. CHAUN. 27, DES VŒUX ROAD CENTRAL, HONGKONG, From the University of Pennsylvania, U.S.A. Hongkong, and January, 1904.

DENTISTRY.

SUI SANG. (Lately Prictising with Dr. I. SAKATA), DENTIST, No. 26 Connaught Road Central.

> TSU FAN DENTIST.

H nekone, oth February and

PRICE MODERATE-CONSULTATION FREE. Next to the Hongkong Dispensary, 50, Queen's Road, Central. Hongkong, 5th January, 1904.

THE NEW FRENCH REMEDY

This successful and highly popular remedy, used in the Continental Hospitals by Ricord, Rostan, Johnt, Valpean and others, combines all the desiderate to be sought in a medicine of the kind, and surpasses everything hitherto

medicine of the kind, and surpasses everything hitherto amployed.

THERAPION NO. I in a resource and the property of the prope

where other well-tried remedies have been powerless.

THERAPION NO. 2 for inthe blood, henry, pimples, spots, blotches, pains and swellings of the joints, secondary symptoms, gout, thermatism,
and all diseases for which it has been too much a fashion
to susploy mercury, sarsaparilla, he., to the destruction of
sufferers' teeth and rule of health. This preparation
purifies the whole system through the blood, and thoroughly
eliminates every poisonous matter from the body.

THERAPION NO. 3 for
schaustion, impaired vitality, alequieseness, and all the
distressing consequences of early error, excess, residence in
hot, unhealthy climates, he. It possesses surprising power
its restoring strength and vigner to the debilitated.

THERAPION is sold by the printhe restoring strength and vigner to the debilitated.

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THERAPION is sold by the printhe restoring strength and vigner to the debilitated.

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THERAPION is sold by the printhe restoring strength and vigner to the debilitated.

THERAPION is sold by the prinlimits of word "THEMAYION" as it appears on the Heit'sh
Convenient Stamp (in white letters on a red ground)
alls d to every parkage by order of it a Majority's Hunlimit of the principles of the principles.

Sold by A. B. WATSON & Co., Limited

Sold by A. B. WATSON & Co., Limited. Hongkong, China and Manila.

Intimation:

The ROBINSON PIANO Co., Ltd.



Hongkong, Shanghai, and Singapore.

Hongkong, 29th March, 1904.

Auctions.

GOVERNMENT NOTIFICATION.

Anemia, Throat and Lung Troubles, and DARTICULARS AND CONDITIONS of the letting by Public Auction Sale, to be held on TUESDAY, the 5th day of April, Works Department, by Order of His Excellency the Officer Administering the Government, of One Lot of CROWN LAND, at Bunham Road in the Colony of Hongkong, for a term of 75 Years, with the option of renewal at a GROWN RENT to be fixed by the Surveyor of His Majesty the KING, for one further term of 75

PARTICULARS OF THE LOT. Boundary Measurements. LOCALITY. ft. ft. ft. tt. Bonham Road 86.6 118 86 53.6 5,000 34 1,500

Hongkong, 26th March, 1904.

GOVERNMENT NOTIFICATION.

DARTICULARS AND CONDITIONS of I the letting by Public Auction Sale, to be held on TUESDAY, the 5th day of April, 1904, at 3 P.M., at the Offices of the Public Works Department, by Order of His Excellency the Officer Administering the Government, of One Lot of CROWN LAND, at Lai-Chi-Kok, in the New Territory of Hongkong, for a term of 75 Years, with the option of renewal at a CROWN RENT to be fixed by the Surveyor of His Majesty the KING, for the rest of the term of lease from China or for one further term THE COMMITTEE of the VICTORIA of 75 years if competent for the Government

PARTICULARS OF THE LOT.

of Sale.	Regiutr No.	y L	ocality.	М	Boun	dary	ts.	cats in tre fect.	sal Rent.	pet Prios.
No.	140.			N.	6.	g.	w.	Conte Squar	Amin	Ď
1	w Kowloon ie Lot No. 2.	i	Chi Kok	feet	feet	feet 687	feet 8=6	345,928	3,078	\$0,378
	Hong	ko] ng, 261	h M	arch	 , 19	04.	"	}	[426

SHE! GOVERNMENT NOTIFICATION.

DARTICULARS AND CONDITIONS of the letting by Public Auction Sale, to be held on MONDAY, the 11th day of April, 1904, at 3 P.M., at the Offices of the Public Works Department, by Order of His Excellency the Officer Administering the Government, of One Lot of CROWN LAND at Conduit Rord, in the Colony of Hongkong, for a term of 75 years, commencing from the 10th day of July, 1899.

PARTICULARS OF THE LOT.

SAIC	y No.		Boundary Measurements.				nts in 4 feet.	Rent	Price.
No. of	Registr	LOCALITY.	y.	8.	2	₩.	Content	Anna	Uppet
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	Inland Lot No. 1712.	Conduit Road	92'6 19'	38'9 31'	173. 181.	179	7.555 E. 67.	9đ	1,8
	Hon	igkong, 31	st I	l Mari	ch,	1904	•	1	1 4

GOVERNMENT NOTIFICATION.

DARTICULARS AND CONDITIONS of the letting by Public Auction Sale, to be held on MONDAY, the 11th day of April, 1904, at 3 P.M., at the Offices of the Public Works Department, by Order of His Excellency the Officer Administering the Government, of two Lots of CROWN Foreshore and Sea Bed at Shaukiwan, in the Colony of Hongkong, for a term of 75 years, with the option of renewal at a CROWN RENT to be fixed by the Surveyor of His Majesty the KING, for one further term of 75 years.

PARTICULARS OF THE LOT.

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100	Registr	LOCALITY	.Ma	•	B.	₩.	Contra	1	The Tree	Upper
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-	A		.53%.0	Ŋ.			1			
1	Hot	gkong, 31	et I	Kar	cb,	190	4		MH	

T'HE CHINA & JAPAN TELEPHONE AND ELECTRIC COMPANY, LIMITED.

HONGKONG EXCHANGE, OPEN DAY AND NIGHT.

SUBSCRIPTIONS.

EXCHANGE LINES, \$100 Per Annum. PRIVATE LINES, By Arrangement.

NO CHARGE FOR INSTALLATION.

N.B.-A special charge is made for lines of more than average length.

[425] ELECTRIC SUPPLIES OF EVERY DES CRIPTION IN STOCK.

INCLUDING:-

DATTERIES. CHEMICALS,

FLECTRIC BELLS, TNSULATORS,

IGHTNING CONDUCTORS.

C WITCHES, Telephones,

IX/IRE, &c., &c.,

PRICE LISTS ON APPLICATION.

ELECTRIC BELL INSTALLATIONS. Erected and kept in order.

Estimates given for all kinds of Electrical

Trained Mechanicians sent to Out-Ports to fit

up installations if required. NOTE ADDRESS:-- 2, ICE HOUSE ROAD.

For full Particulars, &c., &c., Apply to

W. STUART HARRISON A.M. INST. C.E.,

Manager Hongkong, and April, 1993

Insurance.

NORTH GERMAN FIRE INSURANCE COMPANY OF HAMBURG.

THE Undersigned AGENTS of the above Company are prepared to accept First Class FOREIGN and CHINESE RISKS of CURRENT RATES,

C BIRMSSEN & Co. Hanykone, this Mie, 1805

Consignees.

PACIFIC MAIL STEAMSHIP COMPANY.

NOTICE.

CONSIGNEES of CARGO per Stramship

"CHINA,"

The above Steamer having arrived, Consignees of Cargo are hereby requested to send in their Bills of Lading for countersignature, and to take immediate delivery of their Goods from alongside.

Cargo impeding discharge and undelivered by THURSDAY, the 31st instant, at 10 A.M. will be landed and stored at Consignees risk and expense. No Fire Insurance will be effected. E. W. TILDEN.

Agent,

Consignees.

NOTICE TO CONSIGNEES.

"GLEN" LINE OF STEAMERS.

FROM MIDDLESBRO', LONDON, AND STRAITS.

THE Steamship

"GLENARTNEY,"

having arrived from the above Ports, Consignees of Cargo by her are hereby informed that their Goods are being landed at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Company, Limited, at Kowloon, where each consignment will be sorted out mark by mark, and delivery can be

be subject to rent. No Fire Insurance will be effected. All damaged packages must be left in the Godowns, and a certificate of the damage ob

tained from the Godown Company within

Goods not cleared by the 5th proximo will

obtained as soon as the Goods are landed.

ten days after the steamer's arrival, after which no claims will be recognised. McGREGOR BROS. & GOW. Hongkong, 29th March, 1904.

NOTICE TO CONSIGNEES.

"GLEN" LINE OF STEAMERS. FROM MIDDLEBOROUGH, ANTWERE LONDON AND STRAITS.

"GLENTURRET,"

THE Steamship

having arrived from the above Ports, Consignees of Cargo by her are hereby informed that their Goods are being landed at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Co., Limited, at Kowloon, where each consignment will be sorted out mark by mark, and delivery can be obtained as soon as the Goods are landed.

Goods not cleared by the 2nd prox. will be subject to rent.

No Fire Insurance will be effected. All damaged packages must be left in the Godowns, and a certificate of the damage obtained from the Godown Company within ten days after the steamer's arrival, after which no claims will be recognised.

McGREGOR BROS. & GOW. Hongkong, 26th March, 1904. [432

CONSIGNEES of Cargo per S.S. "GLEN-TURRET" are hereby notified that owing to the steamer having been on fire during the voyage, a GENERAL AVERAGE BOND must be SIGNED, and 20 % of the value of the Cargo deposited with the undersigned before delivery can be obtained.

McGREGOR BROS. & GOW. Hongkong, 26th March, 1904.

NORDDEUTSCHER LLOYD, BREMEN.

IMPERIAL GERMAN MAIL LINE. NOTICE TO CONSIGNEES.

THE Steamship "PRINZ HEINRICH,"

of the NORDDEUTSCHER LLOYD, having arrived. Consignees of Cargo are hereby informed that their Goods, with the exception of Opium, Treasure and Valuables, are being landed and stored at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Company, Limited, Kowloon, whence delivery may be obtained.

Optional Cargo will be forwarded unless notice to the contrary be given before 5 P.M.,

No Claims will be admitted after the Goods have left the Godowns, and all Goods remaining undelivered after the 6th April, will be subject

All broken, chafed, and damaged Goods are to be left in the Godowns, where they will be examined on WEDNESDAY, the 6th April, Bt 9.30 A.M. All Claims must reach us before MON-

cognised, No Fire Insurance will be effected.

Bills of Lading will be countersigned by the Undersigned. NORDDEUTSCHER LLOYD.

MELCHERS & CO., Agents.

Hongkong, 30th March, 1904.

"BARBER" LINE OF STEAMERS. NOTICE TO CONSIGNEES.

THE STEAMSHIP "SHIMOSA," FROM NEW YORK.

ONSIGNEES of Cargo are hereby informed that Cargo ex the above Steamer arrived per Chenan from Manila to-day. All Goods have been landed at their risk into Messrs. Butterfield & Swire's Wanchai Godowns, whence

delivery may be obtained. No Claims will be admitted after the Goods have left the Godowns, and all Goods remaining undelivered after the and proxime, will be subject to rent.

All Claims against the Steamer must be presented to the Undersigned on or before the 4th proximo, or they will not be recognized, No Fire Insurance has been effected.

Bills of Lading will be countersigned by DODWELL & CO., LIMITED, Agents Hongkeng, 26th March, 1904.

NOTICE TO CONSIGNEES. FROM MIDDLESBOROUGH, LONDON

AND STRAITS. THE Steamship

"MONMOUTHSHIRE," Captain H. N. Vyoyan, having arrived from the above ports, Consignees of Cargo are hereby informed that their Goods are being landed at their risk into the Godowns of the Hongkong and Kowloon Wharf and Godown Company Limited at Kowloon and stored at Consignees risk and expense.

No Claims will be admitted after the Goods have left the Godowns, and all Goods remaining undelivered after the 1st prox, will be subject

All broken, chafed and damaged Goods are to be left in the Godowns, where they will be examined on the 31st inst., at 2.30-P.M.

No Fire Insurance has been effected. Bills of Lading will be countersigned by SHEWAN, TOMES & Co., General Agents.

Hongkong, soils March, 1964,

Agent. Hongkong, 24th March, 1904.



THE PENINGULAR AND ORIENTAL STEAM NAVIGATION COMPANY,

STEAM FOR STRAITS, CEYL**ON, AUSTRALIA, INDIA,** ADEN, EGYPT, MEDITERRANEAN PORTS, PLYMOUTH AND

LONDON. Through Bills of Lading issued for BATAVIA, Persian Gulf, Continental, Ameri-CAN and SOUTH AFRICAN PORTS.)

"BALLAARAT,"

THE Steamship

Captain C. R. Longdon, R.N.R., carrying His disjesty Mails, will be despatched from this for BOMBAY, on SATURDAY, the 9th April, at Noon, taking Passengers and Cargo for the above Ports.

Silk and Valuables, all Cargo for France, and Tes for London (under arrangement) will. be transhipped at Colombo into a steamer proceeding direct to Marseilles and London; other Cargo for London, &c., will be conveyed vid Bombay,

Parcels will be received at this Office until P.M. the day before sailing. The Contents and Value of all Packages are required, Shippers are particularly requested to note

the terms and conditions of the: Company's Bills of Lading. For further Particulars, apply to E. A. HEWETT,

Superintendent.

Frangkong, 28th March, 1904. DAY, the 11th April, or they will not be re- NORTHERN PACIFIC STEAMSHIP COMPANY.

BOSTON STEAMSHIP COMPANY.

BOSTON TOW-BOAT COMPANY. PROPOSED SAILINGS FROM HONGKONG.

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THE SUPERIORESS of the ITALIAN CONVENT, CAINE ROAD, begs most respectfully to APPEAL to the Residents of Hongkong and the Coast Ports, for their kind patronage and support, and desires to state that she will be pleased to receive orders for all kinds of NEEDLE WORK.

Gentlemen's Shirts made to order, and Cuits and Collars renewed on old ones.

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Hongkong, 8th January, 1904.

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to The Manager. The Editor will not undertake to be responsible for any rejected MS., nor to return any Contribution.

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Single Copies Daily, "ten cents; Weekly, twentyfive Ceuts.

i'r Yongkong (Lelegraph

HONGKONG. ATURDAY, APRIL 2, 1904.

PRAYA EAST RECLAMATION.

It will be matter for satisfaction to the Colony generally, and to Marine Lotholders on the Praya East in particular, to know that the negotiations relating to the reclamation of the eastern foreshore of the Island. initiated by the Honourable Sir Paul Chater, CM.G, and acquiesced in by the property owners, have been brought to a satisfactory issue with the exception of those relating to the boundary between the proposed scheme and the Naval and Military properties. These were still in progress at By bind permission of Major Radoliff and the close of last year, having proved to be very protracted and troublesome. It is evident that the Public Works authorities believe in taking time by the forelock. The head of that department in his last annual report, printed in the current number of the Gazette, states that, in consequence of the proposed construction of the electric tramway along Praya East, it was considered advisable to proceed at once with the raising of that roadway to such a level as would admit of the drains and sewers being extended here after across the reclamation to the New Praya front, thus obviating the taking up and re-laying of the trainway at an early date. A contract for the work was let in November, a premium being offered for its speedy completion. Very satisfactory progress was made, and by the close of the year the raising was practically completed from Arsenal Street No. 2 Police Station, whilst a considerable amount had been done on the remaining portion of the road to the eastward. The space to be occupied by the tramway was left about a foot below the finished surface of the road so as to save the filling in and subsequent digging out of material over this area. The first commencement of the actual work of reclamation is looked for with eagerness, since the completion of the scheme will render available a large area of nsuchneeded building land, centrally situated, which is destined to provide accommodation for the working classes of the Colony and so help to relieve the acute congestion of the

LOCAL AND GENERAL.

population now so densely huddled together

in certain districts of Hongkong.

GENERAL Ventris accidentally fractured his leg on the 12th ult. He is reported to be progressing favourably.

VERY extensive quarrying operations are nov carried on, in the New Terrstory, notably at Cha' Kwo Leng Eyeemun and Ngau Tau Kok.

THE Governor of Foochow has sent a quantity of rifles, ammunition and some Tls. 9,000 to the Viceroy of Liang Kwang as a contribution to help him in restoring order in these provinces.

His Majesty the King has not been advised to minates in close proximity to the Aberdeen | they are invariably tender in their treatment. exercise his power of disallowance with respect to the Ordinance to amend the Public Health and Buildings Ordinance, 1903, (No. 1 of 1923),

MR. J. F Boulton has been appointed to act as Assistant Director of Public Works on and from the 30th ultimo for so long as Mr. P. N. H. Jones is acting as Director of Public Works, or until further notice.

An area of about four acres of land, mostly hillside, on the promontory west of Lai Chi Kok, was placed by the Colonial Government disposal of the Naval authorities in connection with the establishment of a to pedo range in that neighbourhood.

A GENERAL map of the New Territory on a ineers, Mr. Li A Ping being the contractor. these functions with the utmost skill and, his Majesty feels for his wife, whose faithful scale of two inches to the mile is now being The total expenditure, including all charges, propriety. Small wonder is it that the Japanese prepared. It will show all streams, roads, was \$115,272.27; the amount expended under children are grave with a gravity far beyond paths, cultivated areas and villages, the names the contract being \$106,146. These figures in their years, and take life seriously, for its

THE number of squatters' licences on the roll at the commencement of last year was 980, representing an annual rent of \$1,429 63

THE Hon. Treasurer of the Alice Memorial and Nethersole Hospital's begs to acknowledge with thanks the following donations to the funds of the Hospitals :--

Taikoo Sports... ... \$52,80 Ko Cheuk Shing 25

H. E. the Officer Administering the Government has been pleased to provisionally appoint Mr. P. N. H. Jones, Acting Director of Public Work, to be an official member of the Executive and Legislative Councils, during the ab ence of Mr. W. Chatham or natil firther

THE Board of Revenue in Peking has but the brillered a 'eas f collecting the taxes partly in currency! The Throne is said to have approved this fla h of financial nerius.

THE firm of An adds and Armstrong have reboats to cost £360,000. This site same firm which supplied the two crossers Nice its and Kasuga for Japan, which pissed through Colombo last month. It looks very much as if Turkey is making preparations for the war for which every Turl is longing.

THE Impetial Chinese Post Offices in Liu Kung Tau and Port Fdward, have been withdrawn from yesterday and all postal business in the dependency of Weih awei will be conductel through the British Age cyat Lan Kung city of Wesh riwei will be retained for the transmission of Chinese correspondence to the interior. The British Agency at Lin King Tauwill open abranch office at Port Edward.

officers the Band of the 93rd Burma Infantry will play at the Hongkong Hotel this evening

from 8 to 9.30 p.m.
BAND PROGRAMME.
March. " The Stars and Stripes for lever" Sousa
Introduction. " and Act Lohangrin " Wagner
Selection " San Toy " Sidney lones
Mazurba. "La Comtesse" Translateur
Selection " The Orchid" Ivan Carell
Naltz " "April more Balten
Japanese Dance
God save the King.

A MAIL paper has the following interesting points about the Siberian railway: Number of stations on complete Fire, 390; staff on fariway, 14,738; number of locomatives, 751; number of possenger carrages, 5,8; number of goods waggons, 7,743; number of mail vans, 33; miles of line in Siberia, 3,559; miles of line in Manchuria, 1,444; Moscow to trautsk, 8 days; Irkutsk to Mancauria (7.) miles half and 50 miles terry), 74 hours; across Manchura, 949 miles to Port Aithur, 77 hours; former east terminus for Vladivoslock, Streniens,; west terminus of Viadivostock railway Khabanovsk; river steamer route to connect Khabarovsk and Stretensk, 1,340 miles; Vlacilvostock to London by rad (7,092 nules); 174 days.

THE JUBILEE ROAD.

Mr. W. Chatham thus describes the Jubilee | Road in his annual report just published.

The road commences at Kennedy Town and traverses the southern slopes of Mount Davis and the adjoining hills to Aberdeen, opening out en soute a portion of the island which has hatherto been practically inaccessible. It is 5 inites long has a width of 16 feet and is designed as a carriage road.

Leaving Kennedy Town, it rises for about a to its constituction being that it should be kept smill gap adjoining a site acquired by the Military Authorities for gun practice, it falls at Docks by joining the old road near sea level, the reason for constructing this portion being the excess vely steep gradients of the latter

serfaced throughout with decomposed granite. of granite masonry, with brick arches faced

mentioned.

BREAKDOWN OF THE W.D.L. - "OLDENBURG,"

DEPARTURE DELAYED.

We have received the following communication, dated 1st instant, from Messrs, Melchers & Co, local agents of the N.D.L.:-

"The agent of the Norddenischer Lloyd at Aden wired on the 30th March :- Oldenburg cannot sail on account of shaft broken probably departure (8th April; and we would therefore kindly ask you to publish that the departure for Europe of the Imperial German Mail s.s. Otdenburg will delayed for about three weeks."

MILITARY LANDS.

I agotiations between the Colonial Government and the Military Authorities, for an silver and partly in gold ander the impression exchange of lin Is are still proceeding. Pendthat they will thereby obtain a supply of the ing a settlement, the D. P. W. reports, the in any sportsmanlike s irit the sailors yellow metal and Le able in institute a gold Military Authorities were allowed into possession of considerable areas at Pinewood Battery, below High West Gap, and at a point on the Jubilee Road, about a mile beyond the Plague ceived an order from furkey for seven torpedo | construction of new Rifle Ranges behind Kowloon City under the supervision of the Royal Engineers and at the expense of the Colony. This is necessary to enable the preent ranges to be relinquished with a view to the formation of the King's Park in Kowloon.

BUILDING ACTIVITY

IN THE COLONY.

In the general remarks, included in his Tau. The Imperial Chinese Post Office in the Treport on Public Works, for 1503 the Director of Public Works writes .- The Naval Yard Extension and Messrs. Butterfield and Swire's shippard works were still in progress throughout the year. In the case of the former, the sea wall and inner-wall or quay forming the pier on the north side of the Basin, a portion of the east wall of the radio and the Murray Pier Extension and roundhead beyond it were all brought above low-water level, whilst the conerdam enclosing the site of the graving dock was completed and the water pumped out.

In the case of the Quarry Bay Shipyard Works, the side walls of the graving dock for more than half its leggth were in progress and the excavation for the remaining portions well advanced, whilst the cofferdam to enclose the site of the entrance was nearing completion. ub-tantial progress was also made with the levelling operations, large masses of took having been excavated and tipped to form the reclamation.

The large blocks of buildings on the Praya Reclamation between Ice House and Pedder Streets were in progress throughout the year. the triangular block at the junction of Des Verux and Chater Roads, designated "Alexandra Buildings," being nearly completed. The south-castern section of Princes Buildings was roofed in before the close of the year. The buildings for the Electric Tramway Power Station, on the east side of a new Chinese theatre was erected near the more than usually amenable to women's Budget on the 19th proximo. Gas Works, West Point. Substantial progress was made with the reclamation at Tsim Sha Tsui between Blackhead's Point and Kowloon

JAPANESE CHILDREN.

judging from the description given of them by a French writer, who tells us some wonderful facts about the babies of Japan. He declares that "they never cry, and never make a noise," bloodshed, malignity, and strife." The record, mile with gradients of t in 20 and t in 30 until and that Japanese children "play without however, goes to explain a constitutional it attains a height of about 180 feet above sea quarrels and without tears." A grave, self- melancholy to which he is subject, and, aware of the destruction of the station, but level; one of the conditions upon which the possessed politeness is innate with these little furthermore, as though to emphasise his Military Authorities withdrew their opposition | people, and they behave with the most irrepro- | gloomy retrospect, and to continue it in his achable sangfroid in the presence of their own lifetime, the disaster on the Khodinsky 150 feet above the sea. Passing through a elders. The love of children is a veritable cult plain during his coronation festivities is in Japan, and the little ones are surrounded recalled. with a care and solicitude that are almost I in 30 until it reaches the 150 feet contour, religious. A story is told of a burglar who had personality is presented in the following story when it is carried level round Sandy Bay to broken into a house, and postponed hi of his meeting with the Kaiser in the Baltic: below Bisnee Villa. Another ascent at 1 in 30 | nefarious purpo e to amuse a baby who was is then made to a gap about 250 feet above sea | playing by itself, and recome so absorbed in level near Sassoon's Villa, at which point his game that only at the entrance of the there is a cross road connecting it with parents did he beat a retreat. Japanese the old road from West Point to Aberdeen, children, we are told, are very fond of fairy Admiral of the Atlantic salutes the Admiral inquiry into the state of the affairs of the which is at a considerably greater altitude. tales, and several of our own familiar figures of the Pacific." The Tsar was placed in a From the gap, it again falls at the same appear in their favourite stories, such as "The difficult position. If he accepted the implicagradient to the 150 feet contour and then runs [Ogre and Tom Thumb" and "Mother Goose." I con the British Foreign Office might have level through the property of the Missions Many of these tales treat of animal, and something to say. If, on the other hand, the Etrangères. Crossing the Pokfulam Stream among their records of talking beasts there is signal remained unanswered, discourtesy to on a bridge of 4 arches, a little further on, it a dialogue between rabbits and crocodiles. the Kaiser was the result. I am informed that joins the old road to Aberdeen, of which a The Japanese "raconteur" shows a kindly length of 800 feet has been utilized after feeling towards animals and an intense love of improving the gradients. Diverging again at nature. Thus it is that children in Japan will No. 10 Bridge, which has been widened con- display a keen interest in natural objects will not f rget the quiet snub adminis ered to siderably, it falls at 1 in 20 and finally ter- especially in flowers and insects, of which last him one grey afternoon in the shallow waters unfortunates, and medical aid was sent for.

Yet the question of "child labour" has as sumed a phase in Japan that is fortunately rarely reached in Western countries, and below the bridge mentioned. The road is heavy work of all kinds is, strange to say, exacted from children to an extent which There are 10 bridges, all of which are built | wou'd surprise our humanitarian ideas. Childactors and dancers, often not more than six with granite, the spans varying from 10 to 17 | years old, are habitually employed in the feet. All bridges and retaining walls are pro- theatres, and fill the roles that we should allot vided with parapet walls of gracite masonry, to grown-up performers, with a finish and whilst other parts of the road are protected by | intelligence that are amazing. The little girliron railings where accessary. The pathway dancers go through an exceptionally severe before referred to commences near the training in gymnastics to fit them for their bridge across the main stream on the road profession. In the factories child labour is from the Peak to Aberdeen and joins the exploited to a considerable extent, children Jubilce Road near the No. 10 Bridge already | being employed for long hours at a stretch at a very low wage, while in public offices they The work was carried out for the Committee | are often engaged as porters and lacqueys at by Messrs. Denison, Ram and Gibbs, Eng. an absurdly early age, although they fulfil

COKRESPONDENCE.

[We do not necessarily endorse the opinions expressed by Correspondents in this column.]

THE ATHLETIC SPORTS.

TO THE EDITOR OF THE "HONGKONG TRIEGRAPH ." SIR,—The Committee of the V.R.C., under whose auspices the athletic sports are to be held on the Happy Valley on Monday, are surely not to be congratulated on their liberality to the men in the Fleet. On every ship on the station a list of events for the forthcoming! meeting is posted, presumably with the object of notifying intending entrants of the events open to them. To the surprise of the men in the Navy all the events, with only a single exception, are reserved to members of certain specified social clubs and the V.R.C. in the Colony. The only event for which the Bluejackets are allowed to compete is a Balfmile Flat race.

Surely if the competitions are got up in H. M.'s navy should not be excluded. There are many good men among them who can run and jump and by whose admission the various events on the programme Hospital. Arrangements were also made for the will be more keenly contested and the flat, hurdle, and bicycle races will furnish more keen competition and greater interest aroused amongst the spectators than the tame events of past meetings were capable of exciting.

I am one, among many, who hope to see that the hint will be taken by those responsible for the getting up of these annual sports and a larger number of events, in future, thrown open to men of the Fleet .- I am, etc., A BLUEJACKET.

Hongkong, 2nd April, 1904

SHORTHAND 2,000 YEARS AGO.

It will, no doubt, come as a surprise to most people that shorthand was known and practised two thousand years ago. Manilius, a contemporary of Cæsar and Cicero, Virgil and Horace, asserts that some system of reporting very similar to our shorthand was in vogue in his days. Writing these words under the influence of Virgo and Mercury, he says they are :-

In shorthand skilled, where little marks

Whole words-a sentence in a single letter lies:

And while the willing hand its aid affords, Prevents the tongue to fix the falling words. It is certainly a novel conception that Cicero's grand orations were committed to paper with as much skill as our modern stenographers boast.

> THE REAL TSAR. DEPRESSING PEN-PICTURE.

An anonymous writer in the March Fortnightly gives a depressing character-sketch of Nicholas II. His estimate is as follows:-

The Terr Nicholas II is physically weak; his nerves are shattered; his will feeble. He is amiable where iron-handedness is essential. He is dependent when a strong individuality is required. He is constantly in subjection first howrington Canal, were nearly completed, and I to one influence and then to another. He is control; and the fair sex has on more than one occasion exercised powerful, if not calamitous, influence upon his life.

Naturally, the history of the Imperial house is suggested as one of the causes which have operated in the formation of the present ruler's characteristics. The details are too well known to be referred to here, and, in spite of them, as Japanese childr n must be quite delightful, it were, the writer asserts that the Emperor Nicholas "is as right-meaning, benevolent, and clement a ruler as though he did not bear upon his shoulders accumulated memories of

A brighter view of the monarch as a dignified

The Kaiser semaphored an effigive message to the Tsar from the upper bridge of his magnificent yacht the Hohentollern. It was couched in the following terms :- " The the Tsar's reply was his own idea. He simply signalled . "Farewell." The laconic rebuff to the German Emperor went round the fleets of

The Tsar dreads war, and the writer of the article attributes this feeling to his mother. The sentiment has not tended to increase his crushed and suffocated by the weight and popularity, and ever since the attempt upon his | density of the coal, life by a Japanese fanatic at O su in 1891-and the unguarded statement by the Crown Prince of Creece, "Then Nickle ran"-there has been a doubt of his personal courage, which the writer suggests is cruelly unjust. He takes no delight in outdoor sports except in cycling, and and h s "amusements,' which are few, include photography and the occasional production of melancholy verse, but he is a hard worker in affairs of 'tate.

The article gives the following painful description of the young ruler's general curroundings:—

The atmosph re of suspicion and gloom produced by the constant sense of impalpable danger overshadows the spirits of everyone at the Russian Courts . . . The Tsar never knows who are his friends and who his enemies. An abiding sen e of distrust is the resu't, and hence the passionate affection that ounsel and patient sympathy are the bright spots in one of the sadde t lives in Europe.

There is considerable pathos in the terse of the latter being marked, whilst the hills will clude the sum of \$534.25; which was paid by builden falls early enough upon their small The Tear is a kindly, saddened, overworked, and unhappy man,"

TELEGRAMS.

"HONGKONG-TELEGRAPH" SERVICE.

THE WAR. SINKING OF A PRESS

BOAT BY THE RUSSIANS.

CREW CAPTURED. THREE ESCAPED.

(From Our Own Correspondent.)

Wein liwer, 31st March, 3.16 p.m.

When near the Miaotao Lighthouse, on the 26th inst., the Russians fired upon and sunk the s.s. Hanyei, chartered by the Osaka Asashi to serve as the despatch boat for that

The crew were captured, with the exception of three who made good their escape.

> THE " MANDJOUR" QUESTION

> > AT SHANGHAL

(From Our Correspondent.)

SHANGHAI, 31st March,

6.20 p.m. The Mandjour question is now settled, the dismantling of the vessel having been completed to the satis-

faction of the Japanese Government. The Japanese cruiser Akitsushima left this morning.

In our issue of the 29th ult., we stated that, according to a Peking despatch, the Japanese Minister, Mr Uchida, had informed the Waiwupu that his Government was not satisfied at the way the dismantling of the Mandjour in Shanghai had been conducted. In other words, it was complained that only the breech-blocks of the big guns and the ammunition had been taken out of the vessel, while other most important parts of the machinery, etc., on board were not touched. Under the circumstances therefore the Japanese Government did not consider that the Mandjour had been actually made harmless.

***The above telegrams were issued in an extra " yesterday forenoon.-Ed., H.K. T.)

> (Reuter's.) Parliamentary.

LONDON, 30th March. Sir Michael Hicks-Beach has informed

seek re-election to Parliament. The House of Commons will adjourn on the 12th proximo.

his constituents that he does not intend to

Mr. Balfour will probably introduce the

The War. The Destruction of the Russian Quarantine Station at San-Shantao.

Russia having protested, through the French Minister at Tokio, against the destruction of the quarantine station at San-Shantao during the fourth attack on Port Arthur, as a violation of the Hague Convention, as previously telegraphed, Baron Komura, replying, said he was not the Hague Convention did not apply to Naval bombardments.

The French Navy.

In a lengthy debate on the condition of the French navy, M. Lockroy sharply criticized the administration of M. Pelletan. The latter repeated the points of his defence on the 16th instant. M. Combes supported M. Pelletan. The Chamber eventually passed a resolution promising that a special fleet will be instituted.

AN ACCIDENT AT TAIKOO.

A regrettable accident occurred yesterday evening at 9.20 at the Taikoo Sugar Refinery. Three coolies, who were working at the stacks, supplying fuel to the boiler house, were suddealy buried under a slip of descending coal. When the bodies were recovered, life was found to be extinct; the three men having been

SHIPPING AND MAILS.

MAILS DUE. French (Annam) 4th inst. Indian (Hangs ing) 4th inst. American (Dorle) 5th inst. Canadian (Athenian) 7th inst. Chnadian (Empress of India) 15th inst. American (Siberia) 17th insti-Canadian (Tariar) 23nd inst.

The C. & M. Co.'s as. Zafiro left Manila on yesterday the afternoon and is due bere on Monday at daylight. The Silk ex C. P. R. Co.'s a.s. Tartar left

Vancouver B C. for Hongkong via vaual Ports of Call, on Thursday the 31st March. The Imperial German - Mail s.s. Seydlin which is he e on the 20d inst, at noon, has a air ved at Genos, on Wednesday at 8 p m.

The C. P. R. Co.'s s.s. Askanian air ved

Kohe at 3.30 p.m., on Wednesday, the soth Varch, and laves againmat 830 p.m.; on Thursday, the 31st for Shanghal where she in is due to arrive at noon, on 4th inst

IMPORTANT DECISION. On the 29th ultimo, the Japanese Diet, by a unanimous vote, adopted the following motion: - Considering that, in her war with Russia, Japan is struggling in a just cause, the Imperial Diet accepts, with a few trifling modifications, all the propositions of the Government relative to the financial measures rendered necessary by the present conflict.

THE "MANDJOUR".

The following is a note addressed to the Japanese Consul-General, Shanghai, by Sin Wan Pao (28th ult.): Referring to your letter, requesting that prompt measures be taken to remove the navigating machines, guns, and fire arms from on board the Mandjour, question has been delayed, I beg to state that ever since telegraphically instructed by at the Malgakanda Temple the Wai Wu Pu to find a suitable place for the armaments with the assistance of the Commissioner of Customs, many letters dwelling on the subject in question have been exchanged, which are now on file. As a Customs House, and the native junks were not fit for the purpose, it was then resorted to the late Fu Chi of the Kiangnan arsenal, hut it was sold long ago. Under such circumstances I finally wired the Minister of the South, requesting him to send the Nanjui down here, where, after examination, the armament will be stored and scaled by the Commissioner of Customs and a special Mandjour had been fully disarmed or not I did not know for certain until yesterday when, in reply to my urgent letter, I heard from the Commissioner that the Russian Consul-General here expressed his objections based on the telegram received from his Minister, which did not fully set forth the point in discussion, and that in case of emergencies such as storm the ship would be placed in a precarious condition when her navigating machines are removed. and the Russian Consul-General sent two long telegrams to Peking yesterday, addressed

Trans. The following telegrams are from the A'. C D. News of the 28th ult.:-

to the respective authorities: Besides writing

to the Commissioner again on the dismant-

ling of the Mandjour, I beg of you to order

the Atkitsushima to leave accordingly.—

THE LAST ATTACK ON PORT ARTHUR. Tokio, 26th March.

It is concluded, in reference to the engagement at Port Arthur on the 22nd instant, that the steaming out of the harbour of nine Russian men-of-war may be taken as indiciting the revival of martial spirit in the Pussian squadron since the arrival of Admiral Makaroff but the gallant Admiral did not dare to engage the Japanese seriously with vessels incompletely repaired.

Admiral Togo also had no desire to come to close quarters with the Russian squadron while the latter was under cover of the land batteries An indecisive result was inevitable.

The bombardment from Pigeon Bay by the Fuji and Yushima is believed to have had the effect desired, and it is apparent on a general view that the Russian ships are not in a position to do more than defend Port Arthur.

It is believed that the Russian squadron which steamed out to the outer harbour conisisted of the battleships Perseviel (12,700 tons), Petropzulosk (11,000) Pobieda (12,000), Poltava (11,00), and Sevistopol (11,0.0), and the ciuisers Askold (7,000 tons), Bayan (8,000) Diana (7,000), and Novik (1,000).

FINDING THE FUNDS. Tokio, 26th 'arch.

The House of Representatives to-day adopted completely the financial war programme. Regarding the compensation to the merchants concerned in consequence of institution of the tobacco monopoly it was feared that a scandal would ensue, and it was circulated among the members that the position of Mr. Matsuda, the President of the House of Representatives, was threatened, but the trouble was tacifully averted.

The method of compensation was modified to a proportion of the gross proceeds for three years.

THE OFFENDING "NIROKU." Tokio 20th March

Mr. Akiyama, the proprietor of the Niroks Shimbun has voluntarily resigned his seat the House, on account of the defamator; article in his paper referred to in my telegram of the 22nd inst.

THE GENEROSITY OF THE EMPEROR OF

KOREA. Tokio, 27th March. The Emperor of Korea has contributed 100,000 yen to the Japanese Soldiers' Relict

Society. A POSS BLE SIDELIGHT.

Gener 1 Dragomiroff, a noted military authority, was summoned to St. Petersburg to as: is! in the war deliberations on March 5th. He severely criticised the policy pursued in the Far East by Russin, and insisted on the evacuation of Port Arthur in order to avoid greater dis sters. His advice is unwelcome, and has been energetically repudiated.

In spite of the severe bombarding Port Arthur is being subjected to, theatres and restaurants are open as usual with bands each, carrying a load of 14 tons in each truck.

playing in the park. Sir Robert Hart has required all Russians numbers given above may be nearer the truth, and Japanese in employ of the Imperial Maritime Custoins to sign declarations of strict neutrality during the Russo-Japanese war,

It is said the Russians have landed 36 siege guns of different calibre for the defence of Newchwang and neighbourhood. Preparations to contest the landing of the Japanese go on apace, and heavy guns are being mounted at the mouth of the river. Five regiments under General Kondratovich have been told off specially to hold this part. It is true that Japanese men-of-war have been teen outside. Newchwang.

BUDDHIST PRIESTS FOR THE FRONT.

Bhikshu Rio Un Mukoyama Suguna, one of Japanese pupil-monks of the Ven. H. Sumangale, Chief High Priest of Adam's Peak and Principal of Widyodaya College, Colomba, left Colombia for Japan by the last German boat. He will at once proceed to the scene of operations in the Fa East, where he will be Yuan Taotai of this port, as published in the | engaged in ministering to the sick and wounded, and the dying subliers of the Japanese armies. Mr. Suguna will repr sent the Buddhist priesthood of Ceylon. He is the first priest to go to the scene of operations in the yellow garb of Ceylon lihikshu. There are and an explanation why the settlement of the the other ordained Japanese monks under the High Priest, who are all residing at present

A TERRIBLE FLIGHT OF LGCUSIS.

EXTRAORDINARY DETAILS.

An amazing account, which all reads like a suitable place could not be found in the bit of wild romance, is published in the current number of the Bunhay Cathelic I x miner of an extraordinary flight of locusts which visited Satara recently.

Somewhat similar accounts of this strange and unprecedented visitation are given by other correspondents from t e surrounding districts. the one which we extract is signed by II. Lithos, and dated Satara, February 23rd :-

Last week there passed over Satara, the headquarters of the district, two or three swarms of man appointed by the Minister. Whether the locusts; but they were only the precursors of the terrible flight which passed through this station on the 20th and 21st inst.

locusts made their appearance, going from east and myraids of these red creatures began to settle down to rest for the night. No one, however, had the slightest notion of the magnitude of the swarm or the extent of the damage they would cause during the night. The first report on Sunday morning was that the roads were blocked up by branches of trees failing across | rowed or wish to borrow from Europe." "The them, that the tongus from the station were letained, and that traffic was at a standstill; and the cause of all this was the locusts.

AN APPALLING SIGHT.

Every one regarded these reports as proposterous and laughed at these wild stories. At 7 a.m. the locusts started on their ownward course of des ruction, taking the same direction as on the previous evening. It was then that one realized the awful immensity of the flight. As the day advanced the siteam swelled. The main body, thick enough to give a good shade, occupied about one mile in breath the two wings a mile and a half each. The extent of the swarm breadthwise was about 4 miles. A per- Again, the Japanese have not adopted Eurofect sheet of red locusts on the ground, a thick | pean costume. Nor do they mean to. State cloud of them passing overhead, covered by these destructive creatures, the whole place was one mass of red insects.

in their fields to drive the locusts away by beating tine and making a hideous noise enough to | Japan. "The Japanese consider our Eurofrighten the devil, but they shouted and shouted until they could shout no more, and sat down exhausted gaping at the vastness, the immens- | the Japanese wear no jewels." Then follow ity, the infinity of the flight. It was a sight truly appalling.

During Sunday large trees—mango trees, wild fig-trees, pipuls, etc., varying from 10 to 20st. and even more in circumference-trees that had withstood successfully many a violent storm-greaning under the weight of the locusts, yielded like rubber and came down with a crash. But what is most surprising, nay, almost incredible, is that babul trees, which resist the sharp are of the woodfeller, and even cactus, were not proof against these creatures.

There is not a single babul tree in the course followed by the locusts but is a monument with its branches hanging down broken, bearing testimony to the frightful devastation caused by

I can assure such of the readers of the Examiner as may be disposed to doubt the veracity of the above account that I myself had considered it chimerical and could not believe my own eyes even when I had visit d the

NEARLY THREE MILLION TONS OF LOCUSTS. At a most modest estimate the locusts were flying at the rate of about 7 miles an hour, and s they took 8 hours in passing they must have space for each locust, they amount to a great deal more than a billion, that is to say, an arra of 13 figures. It is impossible for any one to easy to speak of them it is quite a different thing when we try to comprehend them. We every minute and hence 86,400 a day. At this (which I may describe as a sort of civilized and another instance—this time a commercial one. The locusts were found to weigh on an average 6 to a tola. Their to al weight would therefore be about 2 millions and 91 thousand tons. If, after compressing them well, we had to transport them by rail it would require 3.0.0 G. I. P. Railway goods trains of 50 waggons

Ten times or possibly a hundred times the but what I have here endeavoured to do is to convey some slight idea what this gigantic flight was like,

A POINT OF ORDER.

At a recent meeting of the Legislative Council of Colombo, II.E. the Governor (Sir. partments of State upon the European model Henry Blake), referring to the firs' question on the Agenda, in the name of the Ron. the Mercantile Member, said le was afraid the whole of the question could not be permitted, the rulers and people of Japan realised "the as, according to the Standing Orders, in any question, no argument or opinion was to be | brute force; "they saw that unless they made offered. The purpose of a question was to obtain information and not to express opinions Nor should a question contain arguments, inferences, implications, and ep thets. He believed the hon, member would see that the later part of his question came within that category, inasmuch as it offered an opinion.

Mr. Figg: I submit to Your Excellency's ruling, and I would ask that the first portion of the question be put.

THE NEW JAPAN.

THROUGH FRENCH EYES.

A 'YELLOW RENAISSANCE,"

M. Feliceen Challage's article, "The Europeanisation of Japan," in the new number of the Revue de Paris, is of exceptional interest at this moment. In the first place, the substance of the article belies the title. M. Challage shows that the Japanese hate and despise what Enrope is pleased to call her civilization; and that their borrawings from Europe have been effected against the grain. and for the express purpose of beating vulgarising Europe with her own weapons, driving her influence in the East. Only her vanity, says M. Challaye, has led Europe to fincy that the 'apanese regard Western civilization as superior to their own, and that, therefore, they are "Europeanising" themselves purposely. The Western diplomatist who described modern Japan as a "bad translation" of Europe merely shared the delusion and conceit of foreigners generally. En opean residents in Japan, says the author, know nothing of the On the evening of Saturday at 4 o'clock the country and its people, not ing outside their places of business. M. Challaye has gone about to west; and as the evening approached myriads | among the Japanese towns and country places, lived with the people, conformed to their manner of life, and so learned to understand them. And for the Japanese M. Challaye has a great love and admiration.

To say the least, "the Japanese have retained more of old Japan than they have boreveryday life of the home -- is still unaffected by Europe." M. Challaye then writes a charming account of the Japanese home, with all its simplicity and fine taste—seldom lacking even in the poorest. These qualities the Japanese have not borrowed from Europe. Between the houses of the rich and those of the poor, there is a kind of resemblance—of equality--which is anything but European; and the reason is that taste is a universal quality among the Japanese. "In no country, is William Morns!: golden rule, "having nothing about you but what you know to be useful and believe to be beautiful,' so scrupulously followed as in Japan." officials wear European dress merely because they have to be in constant communication with representatives of foreign States. In pri-There were people young and old crowded vate, they exchange their frock coats and trousers for the graceful, easy attire of old pean costume inconvenient, laughable, and absurd." "Unlike Europeans and savages several pages on the three great virtues of the Japinese-cleanliness, politeness, and TREES COLLAPSE WITH WEIGHT OF INSECTS. | gaiety. As regards the first, I prope has much to learn from Japan. "The politeness of the Japanese—a sign of 'equality'—the politeness of this hierarchical East is in sur, rising contrast with the aggressive rudeness of our democratic communities. Gentleness, cordiality are the Japanese rule. No scenes of violence. The readiness with which a propeans fly into a passion stupefies the Japanese, and appears to them to be a sign of mnate coar, eness. . .

In his consideration for others a Japanese habitually refrains from making any show of his personal sofrows." I can only refer the reader to M. Charlaye's pages on the quiet garety and the refinement of the Japanese people's ordinary recreations, of their intense appreciation of na ural beauty, their love of country walks, and their association of popular festivels, "not with memories of bloody combats, as in Europe," but with the changes of the year.

TAKING STOCK OF CHRISTIANITY. A Japanese acquaintance of min-, in one of his homorous moods, described to me how a Japanese Commissioner was dispatched to Europe to take stock of its Christianity, with rates. a view to deciding whether the religion, as occu, ied about 274 sq. miles. Where the well as the ironclads, of Europe should be less than 20 to 30 dee, ; i.e. 20 to 30 layers of sioner was so appalled by the conflict between Office in Shanghai on the 21st April. The average and allowing two square inches of he returned home and presented homself to 21st proximo, both days inclusive. the Mikado in Council he replied to his questioners with a slow, sad shawe of his head. Whereupon, said my Jap, "we let the matter count these figures; for though it may seem | drop quietly, and we have stock to our old | \$5171. In London, the stock has ricen to £527 ·hinto religion ever since." I am reminded of Nationals are in request at \$ 54. my I panese friend's irony when I read M shall, therefore, employ the clock to do the Challaye's passages on Japanese morals and counting for us. The clock makes 60 ticks on the three Japanese religious—Shinto sm rate our clock would take more than 35,643 developed animism, wherein nature and self, quoted at \$135. Cantons continue weak at \$175. years to accomplish this task. Let us take the dead and the living are indissolubly connected), Buddhism, and Confucianism-the second of which, non-theological, non-metaphysical, is little more than a rule-of-thumb code of practical morality. Buddhism of a reformed order-a soit of Protestant Buddhism -is making great progress in Japan. An interesting fact this is if taken in connection with (alleged) lead assumed by the Chinese and Ja anese Buddhist hierarchy in what is called the Pan-Mongolian movement. The slightly easier and can be obtained at \$22. Christian missionaries, writes M. Challaye, make no way in Japan-though they have

perfect liberty to teach and preach.

THE REVENCE OF THE EAST.

But as a matter of fact, you say Japan has borrowed from Europe, re-creating whole de-Yes, M. Challage replies; when the Americans and the Europeans half a century ago went with their cannon to open the Japanese door. horrible necessity" of meeting brute force with themselves strong in war, in trade, and finance their superiority in morals, art, and religion would not save them from becoming the prev of the foreigner. They saw that without mili tary colonies that have to endure the bruta! protection of European States. To preserve their own civilization they must be independent, and to be independent they must arm themselves and oppose themselves to the West with the weapons of the West. The Japanese have borrowed nothing from Europe except what has made her separte States strong and independent,"

And so the Japanese have become a firstclass Power, by sea and land. "Under the garb of Europe the Japanese have preserved their old endurance, spirit of discipline and self-sacrifice, love of country, contempt for death

. . They are realising their national amb tions. They have succeeded in subjecting Europeans to the jurisdiction of the Native Courts. Europeans are not allowed to own land in Japan. . . . There are Japanese who cherish the dream of a yellow Renaissance under Japan's leadership-who hope that after planting herself firmly in Korea, Japan will come to China's aid, educating her innumer able population, initiating her into Western methods, showing her how to make herself invulnerable by Europe, then, in union with the new China, releasing the Asiatic races from exploiting, oppressive Europe, expelling the Americans from the Philippines, the French from Indo-China, the English from India; realising, in a word, the dream of a Far East for the Far Easterns, under the protection of the Empire of the Rising Sun.

BRUTALISING EUROPEANISM

Was there ever a dream so magnificent: Does M. Challage believe in the possibility of its fulfilment? "If," he writes, "this dream should ever be realised, and Eastern Asia de livered from European supremacy, this would be the greatest blessing which the West her self could derive from her Europeanisation of Japan." But, however this may be, the deliverance would, in M. Challaye's estimation, be a great blessing to the Japanese themselves In M. challaye's view-and if I may borrow from Matthew Arnold an expression applied by him to another country and people-Japan has been barbarised, vulgarised, and brutalised by her importation of Western institutions. Japan has become a military Power on the Christian model and Japan is now, to a great extent, as M. Challage admits, brutalised by the very mintarism" which is the most offensively conspicuous and aggressive element in the life of some European States at this hour. The West has been intecting Japan with the hateful spirit of Jingo-Imperialism. The new Japanese industrialism-European model-is demoralising great numbers of the Japanese population. "The condition of the workmen—and especially of the workwomen—in the Japanese factories is frightful The contrast is terrible between the joyous life of old Japan and the somere existence of the lanourers of Europe. The smoke of factories saddens, poisons the Japanese landscapes; "the brutal regime of our industrial, Europe has begun to crush the spirit of this happy Japanese race, to rob it of lits joy of life."

COMMERCIAL.

Shanghai advices, dated 28th ult., report business done:-Indo-Chinas at Tis. 64 for cash, at Tls. 64 for Ap.il and Tls. 66 July. Farmam, Boyds at 11s, 1394 and 11s, 140 for cash, Tls. 145 for June, Tls. 1466, Tls. 146/1456 for July. Shanghai and Hongkew Wharfs shares at Tis. 1774, Ils. 1764, Tis. 175 for cash, This, 170 for June and This, $172\frac{1}{4}/175$ for July. Tugs at Tls. 50. Lands at Tls. 107. Langkats at 11s. 282/2834 for cash. Tis. 295 for June and and 11s. 2974/300 for July. Astors at \$34 Chinese Engineering and Mining shares at Tls. 6 ' Bearer."

WEEKLY SHARE REPORT.

In their report dated 31st ult., Messrs Benjamin, Keily and Potts write: - .

To-mor ,w being Good Friday, we are printing our circular a day earlier than usual. During the week, the market has shown considerable activity and in most of our principal stocks, business has been done at enhanced

The Fourteenth ordinary general Meeting of shareholders in the Yangtsze Insurance Asswarm was at its densest the locusts were not | mported to the Island Empire. The Comm so | sociation, Limited, will be held at the head them; but taking them only 5 deep on an la "Christian" practice and profession that when I transfer books will be closed from the 11th to

Banks.-Hongkong and Shanghai Banks have considerably improved and after sales at \$610 and \$615 are now inquired for at

Marine Insurance . - Unions have again been dealt in at \$470 and are still in demand. Uhina Traders are wanted at \$56. North Chinas remain unchanged at I'ls 671 and Yangtszes are Fire Insurances. - Hongkong Fires have been

booked at the advanced rate of \$185. China Fires have been, and can still be, placed at \$83. Shipping.-Hongkong, Canton and Macao Steamboats have risen to \$ 61, and are wanted at the price. In Indo-Chinas, an extensive business has been transacted at improving rates up to 19: and the market closes very strong at this figure. The Shanghai quotation is Tls. 66 buyers. China and Mani'as are Douglas Steumships, bave inquiries at \$34 Star Ferries have been disposed of at \$31 (old) and \$18} (new), closing with further sellers.

Shell Transports are quiet at 19/-. We quote Shanghai Tugs at Tls. 47% for the 'ordinary' and Tls. 461 for the 'preference' shares ex the dividend of Tls. 21 and Tls. 14 respectively

paid in Shanghai on the 29th instant. · Refineries.—China Sugars have strengthene their position, and are in request at \$113, after sales at \$110, \$111 and \$112. Perak Sugars have buyers at Tis. 50.

Mining. -There is no business to record in

stocks under this head.

Docks, Wharves and Godowns. -- Hongkong and Whampoa Docks are steady with further sales at \$205. Since the settlement, Farnhams have materially improved, and are in demand at Tis. 145. Kowloon Wharfs are firm at \$93 Hongkew Wharfs have tumbled to Tls, 170. and shares are reported sold in Shanghai at this

Lands, Hotels and Buildings,-Hongkong Lands are quiet at \$145. Kowloon Lands can be procured at \$35, and West Points are out of favour at \$53. Hongkong Hotels have changed hands at \$135. Astor Pouse Hotels have been sold, and have forther inquiries at \$34. Humphreys Estates are unaltered at \$106. Cotton Mills.-Quotations remain the same

as last reported. Cigar Companies .- Sumatras keep firm with

buyers at Tls. 53. Miscellaneous .- Green Island Cements lave been disposed of at \$15. China-Borneos have changed ownership and are in further request at \$8.10. A. S. Watsons are wanted at \$14 and Valkins can be placed af \$63 ex the dividerd of \$1 paid on the 29th instant Elections (old is-ue) have been fixed at \$13: the new shares have found investors, and thereare further it quiries at \$44. Steam Waterboat: are to be had at \$153 Central Stores have been done at \$20. Langkats remain unchanged at Tis. 8.

TO-DAY'S EXCHANGE.

	Setting.
	London—Bank T.T 164
	Do. demand
	Do. 4 months' sight
	France—Bank T. T 2.225
	America-Bank T.T.
	Germany-Bank T T 1.81
	India T.T
	Do. demand
i	Shanghai—Bank T.T73
	Japan-Bank T.T873
	Singapore-Bank T.TNominal
ı	3111 14 14 14 14 14 14 14 14 14 14 14 14

Java- Bank T.T.1067 30 days' sight San Francisco & New York 43 30 days' sight Sydney and Melbourne 1/9 15/11 6 months' sight lank of England rate4 %

OPIUM QUOTATIONS.

To lay's find claims are at today	W
	Per chast
Malwa New	940/970
,, Old	980/1,060
Oldest	1,100/1,160
Patna New	1,325
Henares New (at	1,315
Persian (Paper	850'950

Asvertisenvents.

MEN WANTED. (TT has been truly said that the great want

of the age is men. Men of thought; men of action. Men who are not for sale. Men who are honest to the heart's core. Men who will condemn wrong in friend or foe; in themselves as well as others. Men whose consciences are as steady as the needle to the pole. Men who will stand for right if the heavens totter and the earth reels. Men who can tell the truth and look the world and the devil right in the eye. Men who neither swagger nor flinch. Men who can take a note rapidly. Men who can type a good letter. Men who can keep true and faithful accounts. Men soho are quick at figures. Men who can speak without welgarisms. Men with good memories. Men with good manners, whether at business or in the Social Circle. Men with a knowledge of another language. Men who can have courage without whistling for it, and joy without shouting to bring it. Men through whom the current of everlasting life runs still, and deep, and strong. Men too large for certain limits, and too strong for sectarian bands. Men who know their duty and do it. Men who know their place and fill it. Men who mind their own business. Men who will not lie. Men who are not too lazy to work nor too. proud to be poor. When in Office, the Workshop, in the Counting House, in the Bank, in every place of trust and responsibility, we can have such men as these, we shall have a Christian civilization—the highest and best the world ever saw." Success in life. V failure. The training

given at the Pusiness College, is the solution. Fvery boy and girl leaving School should have a few months at the Studio, Watkins Buildings Queen's Road Central, (near G.P.O.) Hongkong. Warwick Peele, Principal. Circulars,

Hongkong, 2nd April, 1904.

GOVERNMENT BILLS.

TENDERS for SPECIE, BRITISH and MEXICAN DOLLARS, current in this Colony, in Exchange for Sterling Bills drawn at 10 days' sight on the Lords Commissioners of His Majesty's Treasury, London, will be received by the Chief Paymaster, Army Pay Department, until 11 A.M., on the 6th April,

The Tenders to state the total amount (in Pounds Sterling), and the amount for which each Bill should be drawn, but no Bills will be issued for less than £100.

The Tenders to be in Duplicate, and in sealed covers, addressed to the Chief Paymaster, Army Pay Department, and endorsed "Tenders for Government Bills."

The right to accept or reject any or all of the Tenders is reserved. Copies of Forms of Tender can be had on application. GEO. H. FERRIER,

Colonel, A. P. D., " H.M. Treasury Chest Officer. His Majesty's Treasury Office, Fletcher Street,

Hongkoog, 31st March, 1904

TO LET.

/ EIRION, Nos. 1 and 2. IVI BOWRING VILLAS, No. 2.

HUGHES & HOUGH, 8, Des Voeux Road. Hongkong, and April, 1904.

a'unday's Aovertisements.

THIS ADVT. will appear 3 times ONLY, viz.: this issue, next WEDNESDAY, and finally on SATURDAY, the 9th inst. It will be wise to CUT OUT NOW and Pin, Nail, or

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(P.O. Box 343). Near G.P.O. Hongkong, 2nd April, 1904.

No. 525, E.C.

REGULAR MEETING of ZETLAND LODGE will be held at the FREE-MASONS' HALL, Zetland Street, TO-NIGHT, the 2nd instant, at 8.30 for 9 P.M. precisely. Visiting Brethren are cordially invited to attend. Hongkong, 2nd April, 1904.

DOUGLAS STEAMSHIP COMPANY, LIMITED.

FOR SWATOW, AMOY AND FOOCHOW. "HE Company's Steamship

"HAICHING," Captain Hodgins, will be despatched for the above Ports, on TUESDAY, the 5th instant, For Freight or Passage, apply to

DOUGLAS, LAPRAIK & CO., General Managers. Hongkong, 2nd April, 1904.

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OUTWARDS.

	_ , ,,	
FROM	STEAMERS	Duz
GLASGOW and LIVERPOOL	" JASON "	On 5th April.
GLASGOW and LIVERPOOL	"ACHILLES"	On 10th April.
GLASGOW and LIVERPOOL		
GLASGOW and LIVERPOOL		
GLASGOW and LIVERPOOL		
GLASGOW and LIVERPOOL,		On 1st May.
GLASGOW and LIVERPOOL		On 7th May.
GLASGOW and LIVERPOOL	"DARDANUS"	On 13th May.
		•

HOI	LEWARDS.	
FOR	STEAMERS	TO SAIL
LONDON & ANTWERP LONDON & ANTWERP *GENOA, MARSEILLES & L'POOL LONDON & ANTWERP LONDON & ANTWERP *GENOA, MARSEILLES & L'POOL	" IDOMENEUS" " TELEMACHUS "" " IASON "	On 29th March. On 12th April. On 14th April. On 26th April. On 10th May. On 20th May.
	for Liverpool at London Rates.	Ou soul may.

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VICTORIA, SEATTLE, TACOMA, ali PACIFIC COAST PORTS, NAGASAKI, KOBE and YOKOHA	and via "HYSON"	On 19th April.

S.S. "PING SUEY" will leave Victoria, B.C., for Japan and Hongkong on 24th March.

For Freight, apply to

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Hongkong, 31st March, 1904

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SHANGHAI	FOR	STEAMERS.	To	SAII	4.
COOKTOWN, CAIRNS, TOWNS-VILLE, BRISBANE, SYDNEY and MELBOURNE 8th April.	SHANGHAISHANGHAI	LINAN WHAMPOA HOW KAIFONG WUCHANG CHIHLI	th April, th " th " th " th "	11 11 11	4 p.m. 4 p.m.

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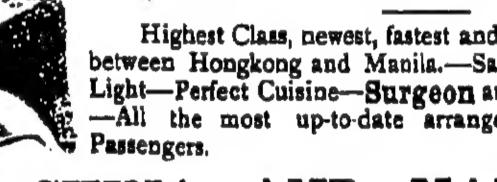
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ZAFIRO RUBI PERLA	2540 2540 1980	R. Rodger	MANILA (DIRECT)	SATURDAY, 9th April, at 10 A.M. SATURDAY, 16th April, at 10 A.M.
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_			
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Through Bills of Lading issued to Pacific Coast Points and all Rastern, Canadian and United States Points. For through rates of Freight and further information, communicate with or apply to ALLAN CAMERON, General Agent.

HONGKONG-MACAO LINE.

S.S. "WING CHAI," Captain SAMUEL BELL SMITH.

PARTURES from Hongkong, on Week

Days, at 7.30 A.M.; on Excursion Sundays, at 8.30 A.M.; from Macao, Week Days at about 2 P.M. and Sundays about 7.30 P.M. FARE :- (Week Days) ist Class (including cabin and servant) 331 Return Ticket, \$5.

and Class, \$1; 3rd Class, 10 cents. On Excursion Sundays, 1st, 2nd, 3rd Class Bingle Ticket, \$2; Return Ticket, \$3. Return Ticket including Tiffin and Dinner either on Board or at Macao Hotel, \$5. On Sundays, \$5 extra will be charged for each cabin with accommodations for two or more passengers.

WHARF-At the Western end of Wing Lok Btreet. The Steamer runs an Excursion Trip EVERY SUNDAY. It takes only 32 hours to reach

Macao. MING ON & CO. and Floor, No. 16, Victoria Street. Hongkong, 5th January, 1904.

Shipping—Steamres.

CHINA NAVIGATION COMPANY, LIMITED.

AUSTRALIAN LINE. REDUCTION IN PASSAGE RATES.

From 1st January, 1904. ALSO REDUC D FARES TO MANILA AND BETURN.

CTEAMERS fitted throughout with Electric Dight, First Class Accommodation. Unrivalled Table. Duly qualified Surgeon carried. BUTTERFIELD & SWIRE,

Hongkong, 1st February, 1904.

MESSAGERIES CANTONNAISES.

I. TREVOUX & CO. HONGKONG-CANTON NIGHTLY

SERVICE. THE Commodious Steamer

"PAUL BEAU," Captain Frangeul, leaves Hongkong for Canton at 9 P.M., on SUNDAYS, TUESDAYS and THURSDAYS, returning to Hongkong the following Days, leaving Canton at 5 P.M., taking Passengers and Cargo as usual.

The S.S. "CHARLES HARDOUIN," Captain Noel, leaves Hongkong on MON-DAYS, WEDNESDAYS and FRIDAYS, at the usual hour.

These Two Magnificent and Up-to-Date Steamers, are lighted with Electricity. The Saloon is under European Supervision. First Class European\$8.00 Second Class European 3.00

First Class Chinese..... 1.50 Second Class Chinese 80 Deck 30 The Company's Wharf is at the end of Queen

Street, Praya West. For further Particulars, apply to J. LANDOLT, Agent,

THE PHARMACY, Queen's Road Central. Hongkong, 23rd March, 1904.

STEAM TO CANTON.

THE New Twin Screw Steamers

Captain "KWONG CHOW"...1,309...J. P. MARTIN. "KWONG TUNG"...1,238...H. W. WAI-KER. Leave Hongkong for Canton at 8.30 Every Evening (Saturday excepted).

Leave Canton for Hongkong about 5 o'clock Every Evening (Sunday excepted). These Fine New Steamers have unexcelled Accommodation for First Class Passengers and are lit throughout by Electricity.

Passage Fare-Single Journey\$4 Meals(Each)

The Company's Wharf is a Short Distance West of the Harbour Master's Office. SHIU ON S.S. CO., LD., and YUEN ON S.S. CO., LD.,

No. 8, Queen's Road West. Hongkong, 17th February, 1904. COMPAGNIE DES MESSAGERIES

MARITIMES. PAQUEBOTS-POSTE FRANCAIS. FOR SHANGHAI, KOBE AND YOKOHAMA.

THE Company's Steamship

"ANNAM," Captain Gerard, will be despatched for the above Ports, on or about MONDAY, the 4th For Freight or Passage, apply to G. DE CHAMPEAUX.

Agent.

Hongkong, 28th March, 1904. BRITISH INDIA STEAM NAVIGATION

COMPANY, LIMITED. FOR STRAITS AND RANGOON.

THE Company's Steamship "PUNDUA," Captain C. Willis, will be despatched as above,

on WEDNESDAY, the 6th April, at Noon. For Freight or Passage, apply to JARDINE, MATHESON & Co., Hongkong, 30th March, 1704

FOR SINGAPORE, PENANG AND CALCUTTA.

THE Steamship

"LIGHTNING," Captain J. G. Spence, will be despatched for the above Ports, on WEDNESDAY, the 6th proximo, at 3 P.M.

For Freight or Passage, apply to DAVID SASSOON & CO., LIMITED, Hongkong, 30th March, 1904.

FOR YOKOHAMA AND KOBE. THE Steamship

"ARTEMISIA," Captain Gronmeyer, will be despatched for the above Ports, on THURSDAY, the 7th April,

For Freight, apply to HAMBURG-AMERIKA LINIE, Hongkong Office: "

Hongkong, 30th March, 1904. [449 RECULAR STEAMSHIP TO NEW YORK,

VIA PORTS AND SUEZ CANAL (With Liberty to call at PHILIPPINE PORTS).

PROPOSED SAILINGS FROM HONGKONG.

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Agents.

NOTICE.

Hongkong, 26th March, 1904.

THE Public are hereby informed that no change has been made in the Rates of Subscription to the Hongkong Telegraph and they are warned against paying more than TEN CENTS (to cts.) per Single Copy,

THE MANAGER, Honghong Telegraph Co., Ltd. Hongkong, 30th September, 1903.

Co be Let.

TO LET.

" RAGSIDE," BARKER ROAD, PEAK. For Particulars, apply to-MURRAY STEWART.

11, Beaconsfield Arcade. Hongkong, 30th March, 1904.

TO LET.

HOUSE in KNUTSFORD TERRACE. Apply to-THE HONGKONG LAND INVEST.

MENT & AGENCY CO., LD. Hongkoug, 28th March, 1904.

TO LET.

NO. I, STEWART TERRACE, Apply to-

THE HONGKONG LAND INVEST MENT & AGENCY CO., LD. Hongkong, 26th March, 1904.

TO LET.

SM 11 1. OFFICE, in Central of DES VŒUX ROAD. Apply to-

"B. C." Clo Hongkong Telegraph. Hangkong, 15th March, 1904.

TO LET.

TO. 6, BARROW TERRAGE, KOWLOOM. Available 1st March.

Apply to-THE SAM WANG CO., LD. Hongkong, 5th February, 1904.

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FOR SALE.

INCANDESCENT GASOLINE LAMPS OF ALL DESCRIPTIONS,

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SHADES, &c., GASOLINE AND GAS LAMPS at the most moderate

prices. Lamps fixed up for Buyers free of charge. Naphtha of the best

kind kept in stock. TAI KWONG CO..

56, Lyndhurst Terrace. Hongkong, 2nd 'April,' 1904.

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"Sanitas" Disinfecting Fluid is non-poisonous and non-staining, and for general or personal use is thoroughly effective. It completely disinfects the house in which it is used, and, administered internally prevents Cholera, T) phoid Fever, Dysentery, etc. **'Sanitas'' Disinfecting Powder**

is the best air purifier known, and a stronger antiseptic and deodorant than carbolic acid, besides being pleasant and refreshing. "Sanitas" Eucalyptus Soap

is specially recommended by the medical faculty for use in hot climates, because of its fine disinfecting qualities and its fragrance. Kingzett's Fumigating Candles

supply the safest and most convenient means of sulphur fumigation. For the disinfection of infected places, bedding, clothing, etc., they are both efficacious and economical. Destroy all insects. THE "BANITAS" CO. LTD. BETHNAL GREEN.

LONDON, E.

WONDERFUL DISCOVERY. This is theage of research and experiment, when all pature, so to speak, is ransacked by the scientific indeed made giant strides during the past century, and among the by no means least important— discoveries in medicine comes that of Therapion, p. rticulars of which will be found in another e denna. This preparation is unquestionably one of the most genuine and reliable l'atent Medicines everintroduced and has, we understand been used in the Continental Hospitals by Ricord. Rostan,

in the Confidental Hospitals by Ricord. Rostan, John Velpeau, Malsonneuve, the well-known Chas-aignae, and indeed by all those who are regarded as authorities in such matters, including the celebrated Lallemand, and Roux, by whom it was one time since sufformly adopted, and that it is worthy the attention of those who requires such a temedy we think there is no doubt. From the time of Aristotic downwards, a potent agent in the removal of these diseases has (like the famed phi-losopher's stone) been the object of scarch of some bone in generous minds; and far beyond the mere body p-if such could ever have been discovered—of in compling the baser metals into gold is surely the overy of a remedy so potent as to replenish the fail in a energies of the comfirmed row in the one case and in the other so effectually, speedly and safely to expel from the system without the aid, or even the knowledge, of a second party, the po sons of acquired or inherited disease in all their projects forms as to leave notaint or trace behind, which is The New Prench Remedy Therapion, which may certainly rank with, if not take precedence of, many of the discoveries of our day, about which no little estention and not clave been made, and the extensive and ever-increasing demand that has been created for this medicine wherever introduced appears to prove that it is destined to east into oblivious all those questionable remedies that were formerly the sole reliance of medical men. Therapion way be obtained in England direct from the proprietor, and of the principal Chemists and Merchanis throughout the Colonies, India, China, Japan, &c., not even excluding such remote districts as Central Africa, the Fill Islands, St., Hrisna, &c., Disseptional Africa, the Fill Islands, St., intimation.

THE HONGKONG TELEGRAPH.

I, ICE HOUSE ROAD, HONGKONG.

CABLE ADDRESS,-Telegraph, Hongkong

HE leading English Newspaper in China Also widely circulated in Japan, Cochin China, Ceylon, India and the Far East generally.

A daily newspaper with weekly edition-[436 published for despatch by the homeward mail The daily is recommended as more generally | Colony. suitable, except for subscribers in Europe or

A special feature is made of full and accurate reports of local occurrences, and of matters of general interest.

ADVERTISING DEPARTMENT.

The Hongkong Telegraph is the best medium for advertising in China. It circulates largely among all classes of the community, is the largest daily newspaper and has a wider circulation than any journal in the Far East.

Special attention given to effectively displayng advertisements.

The type used as a standard for setting believed to be less than 300 miles away from advertisements is similar to this, unless we are instructed to display the advertisement, when any effective style of type will be adopted This standard runs exactly eight lines to the inch, and about eight words to the line.

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(per inch.)

One week.....\$ 2.85 One month 7.20 Three ... 37.50

No charge less than one dollar. Discount allowed on-3 Months Contracts..... 5 per cept.

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can be ascertained from the Manager. Advertisements for the Day hould reach the Hongkong Telegraph Office not later than noon of the day they are intended to appear.

Unless otherwise specified all advertisements will be repeated and charged for until countermanded.

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Job Printing of all descriptions undertaken PRHGRAMMES.

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All job printing is done under European supervision, well turned out, free from errors, and remarkably cheap at

THE HUNGKONG TELEGRAPH OFFICE.

Estimates given for all classes of work on application to: THE MANAGER, HONGRONG TELEGRAPH CO., LD 1. Ice House Road

Houghoug.

HONGKONG METROROLOGICAL

SIGNALS,

A NEW CODE.

We have received from the Hongkong Observatory a new code of meteorological signals which comes into force at Hongkong on New Year's Day. They are the same as those at present in use at Shanghai, and will be hoisted on the mast beside the time-ball a Kowloon Point for the information of masters of vessels leaving the port. They do not necessarily imply that bad weather is expected. The signals are as follows:-

A cone point upwards indicates a typhoon to the North of the Colony.

A cone point upwards and drum below indicates a typhoon to the North-East of the

A drum indicates a typhoon to the East of the Colony.

A cone point downwards and drum below indicates a typhoon to the South-East of the Colony.

A cone point downwards indicates a typhoon to the South of the Colony.

A cone point downwards and ball below indicates a typhoon to the South-West of the Colony.

'A ball indicates a typhoon to the West of the Colony.

A cone point upwards and ball below indicates a typhoon to the North-West of the Colony. Red Signals indicate that the centre is

believed to be more than 300 miles away from the Colony. Black Signals indicate that the centre is

the Colony.

The above signals will, as heretofore, be hoisted only when typhoons exist in such positions or are moving in such directions that information regarding them is considered to be of importance to the Colony or to shipping leaving the harbour.

NIGHT SIGNALS.

Two lanterns hoisted vertically indicate bad weather in the Colony and that the wind is expected to yeer.

Two lanterns hoisted horizontally indicate bad weather in the Colony and that the wind is expected to back. The signals are repeated on the flagstaff of

M's Receiving Ship.

the Godown Company at Kowloon, and also,

by day only, at the Harbour Office and on H

LOCAL STORM-WARNINGS. The Colony itself is warned of approaching typhoons by means of the Typhoon Gun placed at the food of the mast, which is fired whenever

a strong gale of wind is expected to blow here,

Ferry Company's Pier, Ice House Street,

NOTICE BOARDS. Notice boards are placed at :--Joint Cable Companies' Office,

Post Office. Harbour Office.

Blake Pier.

Ferry Company's Pier, Kowloon . WEATHER-FORECASTS and STORM WARNINGS are exhibited on the above boards daily about 11 a.m., and also at other hours. day or night, whenever necessary. Information of importance is also issued by "Express.".

THE CHINA COAST METEOROLOGICAL REGISTER is exhibited at the same places daily about noon. It contains observations made at Hongkong and at a number of stations in the Far East, together with Remarks, Weatherforecasts, and information regarding the existence and movements of typhoons based thereon.

SPECIAL INQUIRIES.

Masters of vessels or their agents may. whenever necessary, call at the Telegraph Company's Office in Connaught Road and send telegrams to the Observatory asking for special information without charge. Such inquiries may also be sent from the Police Station at Kowloon Point which is connected with the Observatory by telephone.

THE LAW OF STORMS

Further information concerning the weather to be expected while signals are hoisted and sailing directions, are given in The Law o Storms in the Eastern Sens.

F. C. Flog.

Action Director

TO-MORROW.

Vessels in Port.

HONGKONG.

THE SHARE MARKET. snipping. Kulka, F. Anderson, Mr. Bading, Mr. and Mrs. Leggatt, E A. St. John's Cathedral, Hongkong. STRAMBRE. TO-DAY'S The following will be the order of service Berner. Miss B. Lewis, A. R. PAID UP Abessinia, Ger. s.s., 3,965, Filler, 31st Mar.,-LAST DIVIDEND. Stocks. Black, Mr. and Mrs. QUOTATIONS Lewis, J. H. April 3rd ; Easter Sunday :--VALUE. Oscar II, Norw. s.s., 2,000, R. Olsen, 31st Mar., Shanghai 20th Mar., Gen.-H. A. L. Holy Communion 7.30 a.m., Matins II a.m., Boggan, Mr. & Mrs. R. -Moji 25th Mar., Coal.-M. B. K. Binandi, Am. 8.5., 273, Diego de Aldamir, 27th Processional Hymn: 131, Responses: Tallis. Bonner, E. A. Macgowan, R. J. BANKS. Huron, Br. s.s., 1,989, W. Walsh, 31st Mar.,-Easter Anthem : Humphreys. Proper Psalms : Bonnet, F. Mar,-Iloilo 21st Sugar.-C. H. O. D. & Mackie, Miss E. Manila 28th Mar., Ballast .- M. B. K. II. (Cooke), LVII. (Rimbault) and CXI. Hum- Borthwick, Mrs. R. W. $\{Div.of \angle 1.10/-\& bonus of 10/-@1/8=1$ Hongkoug and Shanghai, Marriott, Dr. O. Glenfalloch, Br. s.s., 1,434, Bainbridge, 1st Borg, Nor. s.s., 738, Mathisen, 28th Mar.,— April,—Singapore 26th Mar., Gen.—Joo \$22.99 for half-year ending 31.12.1903 \$617 b. phreys. Te Deum: Tours in F. Benedicitus: Brown, W. S. Banking Corporation..... Mast Sidney National Bank of China, Ld.... 3/6=\$2 for 1903...... \$35\ b. Barnby in E. Anthem: "They have taken Buck, Hart Mast, Mr. and Mrs. E. Bangkok 20th Mar., Rice.-Chinese. Teck Seng. Founders ... away my Lord"-Stainer. McAran, T. P. Byworth, Mrs. L. A. Bourbon, Fr. s.s., 997. Antoni, 30th Mar.,— M. Struve, Ger. s.s., 966, P. Brandt, 1st April, Holy Communion 12 noon. Kyrie, Gloria Clark, W. G. McGee, Mr. and Mrs. Saigon 26th Mar., Rice.—Man Fat. -Tamsui 29th Mar., Amoy 30th, and and Credo: Stainer in F. Hymns: 140 and 134. Cobbs, T. F. MARINE INSURANCES. Swatow 31st, Gen. -O. S. K. Carl Diederichsen, Ger. s.s., 774, Schlaikier, Evensong (5.45 p.m.) Responses: Tallis. Colson, F. S. Proper Psalms: CXIII. (Battishill); CXIV. Corlies, Mrs. J. E. Meikle, Mr. & Mrs. E. Union In. Society of C'ton, Ld. \$ 32 per cent-\$32 per share for 1902 ... \$470 sa. & b. Haiching, Br. s.s., 1,267, A. E. Hodgins, 1st 27th Mar.,-Haipliong 24th Mar., Gen.-J. Miller, P. L. 16 %=\$4 for year ended 30.4.1903...... \$56 b. Final of £1 making £2 for 1902 Tis. 67 b. China Traders' In. Co., Ld. ... \$ April,-Foochow via Amoy and Swatow (Tonus Peregrinus) and CXVIII. (Goodson Cotton, Miss Monro, R.N., Com. C. S North China In. Co., Ld...... 31st Mar., Gen.-D., L. & Co. and Walmisley). Magnificat and Nunc Dimit- Coulson, C. Il China, Am. s.s., 3,186, D. E. Friele, 26th Mar., Moore, J. II. Yangtsze In. Association, Ld. 5 20 %=\$12 for 1901 \$135 Kwangtah, Ch. s.s., 1,536, W. H. Lunt, 1st -San Francisco and Shanghai 24th Mar., tis: Maunder in D. Anthem-" Awake up Cowden, Mrs. A. R. Murphy, Mr. and Mrs. Canton In. Office, Ld. \$ 30 %=\$15 per share for 1902 \$175 s. April,-Shanghai 29th Mar., Gen.-C. M. my Glory "-Barnby. Hymns: 136 and 138. Cowden, Miss K. Mails and Gen .- P. M. S. S. Co. S. N. Co. Sevenfold " Amen " : Stainer. Davies, Mrs. J. T. Newington, A. G. China, Ger. s.s., 1,000, Kruebbe, 30th Mar.,-FIRE INSURANCES. Ardandeary, Br. s s., 2,000, W. Kinley, 1st Voluntary: "Worthy is the Lamb" (Mes- Deacon, F. B. North, C. J. Saigon 24th Mar., Rice.-Chinese. April,-Moji 27th Mar., Coal.-B. & Co. Hongkong Fire In. Co., Ld. ... | \$ \$22} per share for 1902 \$285 sa. Osborn, Mrs. F. siah")-Handel. Chowfa, Ger. B.s., 1,055, J. Köhler, 30th Mar., Meefoo, Ch. s.s., 1,321, J. Whenland, 1st April, China Fire In. Co., Ld. \$ 20 \ \$6 div. & \$t bonus per share for 1902. \$83 b. Osborne, E. CHURCH SERVICES. -Bangkok 25th Mar., Rice and Gen.-Derbyshire, J. H. -Canton 31st Mar., Gen.-C. M. S. N. Wesleyan Garrison and Naval Church Douglas, Capt. & Mrs. J. Paget, A. R. B. & S. SHIPPING. Wanchai:-Sunday 10.15 a.m., Sunday Downing, J. C. Pattie, Mr. & Mrs. J. A. Tritos, Ger. s.s., 1,033, Kraft, 2nd April, Foo Shing, Br. s.s., 1,428, T. Arthur, 27th Mar., Pike, R.N., Lt. & Mrs. G. Hongkong, Canton, & Macao 3 p.m. Sunday, School and men's Bible Dowson, A. -Moli 21st Mar., Coal.-J., M. & Co. Swatow 1st April, Gen .- O. S. K. Class Sunday 6 p.m. Thursday 7.30 p.m., Edmonstone, Sir Arch Potter, A. G. Itaura, Br. s.s., 3,363, C. R. Kendall, 2nd April, Gaea, Ger. s.s., 625, H. Dahl, 25th Mar.,-Sai-Steamboat Co., Ld... \$ \$1\frac{1}{2} for half-year ending 31.12.1903 ... \$27 b. and Lady, Maid and Potts, W. H. gon 20th Mar, Rice and Flour.-Chinese. Bible Class. Indo-China S. N. Co., Ld...... £ -Moji 27th Mar., Coal.-J., M. & Co. Regge, W. O. Glenartney, Br. se, J. Stevenson, 29th Mar.,-China & Manila S.S. Co., Ld., \$ 10 X = \$5 per share for 1900...... \$12 The Church which has been closed for the Roche, Hon. Caroline Ellis, Mr. and Mrs. A. Douglas Steamship Co., Ld... \$ Singapore-25td Mar., Gen. -- McG. B. & G. Div. of \$3 for year ended 30.6.1903... \$34 b. last four months for extensive alteratious and Renartales. Rosenthal, H. S. \$1.20 \ - 12% for year ending \$ \$31 sa. & s. Jacob Diederichsen, Ger. s.s., 623. B. Ohlsen, repairs, will be re-opened for service to-morrow 'Star " Ferry Co., Ld } 5 April 1. Rodman, Lt. Comdr. Gale, C. H. 31st Mar.,-Moji 25th Mar., Coal.-O. Easter Sunday. The services are at a quarter 60 cts. 30'4'03 \$ \$18 sa. & s. Sayle, R. T. D. Tremont, for Tacoma. Gertsch, T. "Shell" Transport & Trading past ten in the morning, and at six o'clock in Interim of 1/- for 1903 19/-Progress, for Holhow. Schmidt, W. E. Glover, C. Co., Ld..... evening. The preacher to-morrow is the Rev. Kohsichang, Ger. s.s., 1,299, Spiesen, 29th Rajaburi, for Swatow. Schneeloch, R. H. Grant, A. W. Taku Tug & Lighter Co., Ld. Tls. 50 Final of 2 % making 4% for 1903 Tis. 35 Mar.,-Bangkok 10th Mar., and Swatow T. W. Fearce of the London Mission. Simmers, Mr. and Mrs. Quangnam, for Hongay. Grant, E. W. Shanghai Tug & Lighter Co., Rice and Wood.-B. & S. We are asked to state, for the information of Machew, for Swatow. Sninkher, Mrs. R. W. Graves, Jr., H. C. Limited.....Tls. 50 Final of Tl. 23 making Tls. 43 Tls. 47 hxdiv. Lightning, Br. s.s., 2,122, J. G. Spence, 28th parents living at Wanchai and neighbourhood, Teesta, for Yokohama. Skott, C. Mar.,-Calcutta 12th Mar., Penang and that the Sunday School at the above church Preference Tis. 50 Final of Tl. 14 making Tls. 34 Tls. 46 ex div. Gulert, Dr. von Thales, for Coast Ports. Somerville, Geo. Hall, Capt. T. Singapore 23rd, Gen.-D., S. & Co., Ld. will be conducted at 3 p.m., on the Sunday REFINERIES. Stanley, H. H. Tydeus, for Manila. Hamblin, J. H. Loyal, Ger. s.s., 1,235, Buhnmann, 26th Mar., —Sourabaya 14th Mar., Sugar.—Order. afterucons. Medan, for Sydney. Hamersley, B. F. Stanford, Mrs. L. Fin. of \$7 making \$12 for 1903 \$113 b. China Sugar Refining Co., Ld., \$ 100 St. Peter's Seamen's Church. Batavia, for Kobe. Hammon, Miss Stuart, Capt, and Mrs. Madeleine Rickmers, Ger. s.s., 1,020, Sanders, Luzon Sugar Refining Co., Ld. \$ 100 Queen's Road West. \$3 per share for 1897 \$10 s. E-Sang, for Shanghai. Haughwont, W. B. Leslie C. Perak Sugar Cultivation Co., 29th Mar,-Bangkok 23rd Mar., Rice.-Holy Communion 7.30 a.m. Chihli, for Canton. Swann, G. Hayton, J. T. Matins 11 a.m., Venite, Humphries Te Deum, f,d Tis. 50 5% = Tls. 21 for year ending 30.9.03... Tls. 50 h. Attaka, for Canton. Thomas, O. B. Hemens, R. H. Mausang, Br. s.s., 1,641, S. J. Payne, 25th Mar. ackson Jubilate Russell, Anthem, "What are Hunan, for Canton. Thomas, H. T. Home, Major MINING. -Sandakan 19th Mar., Timber and Gen.these?" Stainer, Hymns, 133, 137, and 376 Hooper, Mr. and Mrs. Thoresen, O. Kyrie, Mendelsolm. J., M. & Co. Trimmel, W. D. None ... 51 #. Punjom Mining Co., Ld. \$ 11 Hudig, D. Nicomedia, Ger. s.s. 2,809, A. Wagner, 21st Amara, for Saigon. Holy Communion 12.15 p.m. Vernon, Mr. and Mrs. J. Société Française des Char-Hutton, M. S. Mar,-Manila 15th Mar., Gen.-H. A. L. Evensong 6.30 p.m., Magnificat, Henly Hongkong, for Haiphong. Interim of Frs. 30 for 1903...... \$500 bonnages du Tonkin Fr. 250 Vogel, R. H. Jina, Br. s s., 3,426, S. Boon, 31st Mar.,-Nunc, Foster, Hymns, 132, 135, and 389. Breid, for Saigon. lackson. Mr. and Mrs. Walppshael, Mr. Raub Australian Gold Mining Oopack, for Shanghai. Karaisu 26th Mar., Coal -I., M. & Co. Anthem " What are these? Stainer. Whitton, Mrs. A. M. Yuensang, Br. s.s., 1,128, P. H. Rolfe, 28th The Church launch Dayspring will call on Joseph, Mr. and Mrs. Wolff, Philip Chinese Engineering & Min-Mar.,-Manila 25th Mar., Gen.-J., M. & PRESCRIPTS STRIVES. ships carrying white crews to bring friends No. 2 of 1/- per share 26.10.03 The 6 sa. Woolmer, Mr. & Mrs. ashore to the services between 9.15 and 10.30 Per Glenfullock, from Singapore-919 Chi-Wright, D. D. Katsch, E. A. a.m., and between 5.15 and 6 p.m., (Kowloon) Docks, Wharves and Godowns. Wright, Mr. and Mrs. Police Pier, 10.30 and 6 p.m.): returning after-SAILING VESSELS. Per Tritos, from Anning-Miss Hastings, Kennedy, E. J. wards. The Answering Pennant is the Call Hongkong & Whampoa Dock Algoa Bay, Br. bq., 1,111, Title, 4th Mar.,-Wright, Mr. and Mrs. and 25 Chinese. Kimball, Mrs. E. B. flag. All the sittings are free and unappropriat-\$6 div. & \$1 bonus for 4-year 30.6.03... \$205 Co., Ld...... \$ 50 Per Kwanglah, from Shanghai-Mr. O. Hongay 15th Feb., Coal.—B. & S. Kirkwood, Miss ed. Visitors welcome. Books, &c. provided. S. C. Farnham, Boyd & Co., Interim of Tls. 5 for t-year ending Ellebrock, Mrs. A. Schmidt, Mr. W. H. Bren-Arrow, Br. 4-masted bq., 2,971, McDonald, Kriglsteir, Baron von Sunday school 10 to 10.45 a.m. nan, and 136 Chinese. 17th Mar,-from New York, Kerosine Oil. Hongkong & Kowloon Wharf Per Hatching, from Coast Ports.-Mr. K. KING EDWARD. & Godown Co., Ld...... 5 Final of \$21 making in all \$5 for 1903 | 594 b. Richter, Revs. Mackenzie, Blocke, G. H. Lyndhurst, Br. masted-bq., 1,879, Parnell, 20th Other 3,3091 \$2\ for 1902 \$37\ s. New Amoy Dock Co., Ld...... \$ Astill, B. W. Kent, Mrs. F. E. Watson, Dr. Whyte, Dr. Alexander, Mrs. A. K. Mar .- Kobe 13th Mar., Ballast-S. O. Co. Shanghai & Hongkew Wharf Lundholm, Capt. B. Scott, Messrs. S. P. Hun, Chas. Kit, W. T. Vale of Doon, Sarawack bq., 669, Wesemeier, Astill, Miss A Mail will close for :-& Godown Co., Ld. Tis. 100 Final of Tis. 6 making Tis. 22 for 2903 Tis. 270 s. Masirevich, S. de Ying, T. Poo, J. Hayasha, and 369 Chinese. Bakovszky, A. de 24th Mar .. - Rajang, N.W. Borneo 18th Canton-Per Falshan, 3rd April, 9 A.M. Muelle, Ed. (Consul for Berry, Thos. Feb., Timber. -S., W. & Co. LANDS, HOTELS AND BUILDINGS. Namtao-Per Taichun, 3rd April, 9 A.M. Boker, Dr. C. J. Peru) Passengers departed. Sanbue-Per Hot Fu, 3rd April, 9 A.M. Ough, Mr. & Mrs. A. H. | Hongkong Land Investment & Brown, Mrs. S. E. Per Rubi, for Manila-Mr. and Mrs. F. H. Macao-Per Wingchai, 3rd April, 9 A.M. Steamers Expected. Final of \$5 making \$12 for 1903 \$145 s, Agency Co., Ld, Brown, Kensett Patin, G. Hyndman, Messrs. A. W. Chront, Chas. Chin, Namtao-Per Taichun, 4th April, 9 A.M. \$2.60 per share for 1903 \$35 s, K'loon Land & Building Co., Ld 5 Dayes, Mr. and Mrs. A Pedersen, S. G. G. F. Richmond, A. M. Clover, Lt.-Comdr. Sanbue-Per Hoi Fu, 4th April, 9 A.M. Rose, Mr. & Mrs. T. J. West Point Building Co., Ld. Final of \$1.70 making \$3.20 for 1903... \$53 s, Eibsloh, A. Agents B. Milton, Messrs. L. Montes, W. S. Hang, W. From Vassels Macao-Per Wingchai, 4th April, 9 A.M. \$5 for 2nd 1-year 1903 \$135 sa. | Hongkong Hotel Co., Ld. ...| Ehrhardt, Capt. W. Sellner, Mrs. Geo. Leo Sing, Mr. and Mrs. A. W. Rettiy, Messrs. Shanghai-Per Whampon, 4th April, 3 P.M. Stephens, Mr. & Mrs. Astor House Hotel Co., Ld.... Gibbins, W. B. T. Fukuchi, G. Patin, Tom Ling C. S. Pri. Rov. Europe, & C. Dictic, vin Turscorin - Per 21 % for year ending 30,6.03 534 b. (Shanghai) \$ Gibbins, Dr. H. B. W. Healy, V. Chong, Wm. Khun, H. F. La-Armand Behic, 5th April, 11 A.M. Hotel des Colonies Co., Ld. Talati, Mr. and Mrs. M. Jameson, J. Watt belle, E. Smith, A. W. Seibert, W. F. M - Annam Saigon M. ... Shanghai, Nagasaki, Kobe, Yokohama, 6 % for year ending 31.3.03 Tis, 15 sa. (Shanghai)Tis. 25] Kent, R.A., Lt.-Col. F. Laughlin, W. Tunerpi, L. S. Sway, Mrs. E. I. Zafiro Manila ... S. T & Co April 4 Honolulu and San Francisco-Per China, 5th Humphreys Estate & Finance Vaughan, H. S. Munson, Messis, H. Komoho, H. K. Hemans, Hangsang Singapore if M. & Co. April April, 11 A.M. Co., Ld. 5 C. C. Su, Chas. Viteral, I. Moy, Chin - Macin, | Capri Singapore . C. & Co.... April Straits and Rangoon-Per Pundua, 6th (Final of 6 % & bonus of 4 % making ? C. Hong, F. Cairs, Lt.-Comdr. W. S. Hugher, Dor c Japan P. M. Co., April Shai Land Investment Co., Ld. Tis. CONNAUGHT. 16 % for 1903 April, 11 A.M. Mr. and Mrs. H. B. Dernell, Mrs. Ofusa-Hori, Artemisia Singapore ... H. A L. ... April McKilligins, H. G. Singapore, Penang and Calcutta-Per Light- Allen, W. E. COTTON MILLS. Tjimahi Moji C. J. J. L. April Messrs. L. S. Yue, C. C. Sam and J. J. Ryan. ning, 6th April, 2 P.M. Bell, Mrs. J. Moir, Geo, A. Per Prinz Heinrich, from Hongkong for Athenian Japan C. P. R. Co April Manila-Per Kaifong, 6th April, 3 P.M. Newborn, R. H. Bell, J. F. (Final of 60 cents, making \$1 for) Hongkong Cotton Spinning, Formosa Singapore P. & O. Co April 7 Yokohama and Kobe-Per Artemisia, 7th Boyce, W. B. . Shanghai-Messrs. H. Firge, A. von Bohr set Olegario, G. Weaving & Dyeing Co., Ld. 5 10 Christie, Mr. & Mrs. D. Pinson, A. V. zewiez, R. Walter, A. G. Wood, I. Talam, A. April, 11 A.M. Ewo Cotton Spinning & Indravelli Portland .. P. & A. Co April 13 Jesus, W. O. Pegge, Jeltes, R. Maclean, P. A. lloilo-Per Wuchang, 7th April, 2 P.M. Rees, G. C. Cronin, John Weaving Co., Ld. Ils. 50 8 % for period ended 31.10.1903........ Tis. 31 %. Emp. of India.. Vancouver. C. P. R. Co April 15 Swatow and Tientsin-Per Chihit, 7th April, Davis, F. O. Frbstoh, F. M. Britto, Rev. and Mrs. G. Davis s. Roberts, A. G. International Cotton Manufac-Siberia San F'cisco P. M. Co... April 17 3 P.M. Robertson, W. R. Dr. G. Boker, Mrs. Rohweder, Hon. and Mis. Dufour, Mrs. B G. Stewart, Mrs. Jesus, Mr. and Mrs. Fontana, Tartar......... Vancouver. C. P. R. Co April 22 Interim of 3 % on account of 1898 ... Tis. 25 b. Rutherford, N. H. Manila—Per Zafiro, 9th April, 9 A.M. Dulot, Mme. Laou-kung-mow Cotton Spinn-Mrs. R. B Snitcher, and Miss Astill. For Europe. &c., India, via Tuticorio-Pei Eyre, Mr. and Mrs. H. ing & Weaving Co., Ld...... Tis. 100 Interim div. of 4 % on acct. of 1898 ... Tis. 324 sa. Nagasaki-Messrs. Meyasake, Danna, Mid-Ballaarat, 9th April, 11 A.M. Swaby, Thomas C. Harris, J. D. Say Chee Cotton Spinning Trimingham, N. S. P. sumote and Imamura. For Kobe-Dr. Ritter Singapore, Penang and Bombay-Per Capri, Hayter, L. Co., Ld. Tis, 500 4 % for period ended 31,12,1897,...... Tis. 170 s. Victor va Bane", Mr. and Mrs. Orabar, Messrs. Heckford, R. G. Wakeman, G. H. 13th April, 11 A.M. Williams, W. H. CIGAR AND TOBACCO COMPANIES. Europe, &c., India, via Tuticorin-Per Helme, E. B. Nomura, Neshide, Tonohova and Kanwasima. April : at April : at Young, W. R. For Yokohama-Messrs. O. von Rokowsky, B, Hamburg, 13th April, 11 A.M. Howard, R. 10 A.22. 4 p.m. Kempthrone, A. B. Manila-Per Rubi, 16th April, 9 A.M. de Masirevic, M. M. Xavier, Mrs. M. M. de Philippine Co., Ld. \$ 10 First year \$10 b. Shanghai, Nagasaki, Kobe, Yokohama, Mendonca, Mrs. Silva and child, Mrs. Silva, Bardineter 30.14 Shanghai - Sumatra Tobacco Victoria and Vancouver, B.C.—Per Athenian, Messrs, S. Meshigori, Chung Mol. Chiu Lon, 1 ethperature 64 PEAK. Wong To, Wong Kow, Dr. Halller, and Mr. Hnmidiev 57 27th April, 11 A.M. McDermott, A. P. B. Ayres, H. and Mr. and Mrs. Schiblye. MISCELLANEOUS. Books containing stamps of the following Mortimore, Miss E. Raipfall. --Morris, Dr. and Mrs. denominations may be obtained at the counter Beattie, M. P. 12 %=\$1.30 per share for 1902 \$25 sa. Green Island Cement Co., Ld. 5 of General Post Office for \$1.00 each. Mozon, Mr. and Mrs. Bensen, Major & Mrs. Hongkong & Whampon Dock Returns. China-Borneo Co., Ld....... \$ 60 cents per share for 1903 \$8.10 sa. & b. CHINA COAST METEOROLOGICAL REGISTER . 16 stamps at 4 cents. Benson, Mr. & Mrs. Herbert Interim of 5 % for 1903 \$14 b. Hanoi at Kowloon Dock. A. S. Watson & Co., Ld. \$ March 31st, 1904, a.m. Bunny, Major and Mrs. Ollis, Mr. and Mrs. Watkins, Ld. 5 Lin Tan Bunny, Miss Pollock, H. E. Bar. Th. Hu. Wind. Wr. China Provident Loan H.I.G.M.S. Moewe ... Until further notice the transmission of Brawn, Col. L. F. Post, N. 8 %-Bo cents per share for 1903 So Mortgage Co., Ld. Mercedes correspondence via Dainy and the Trans-Pratt, Major and Mrs. Chapman, Mr. & Mrs. Vladivostock. 7 a.m. 90 cents for year ending 30.4.1903 ... SES Hongkong Electric Co., Ld. Tak Hing Siberian Railway is discontinued, Chichester, Major and Qurreaunse, Capt. Nemuro 6 a.m. 30.16 -45 cents for year ending 30.4.1903 ... \$7 Hongkong Electric Co., Ld. H.M.S. Otter Cosmopolitan v. w. Parcels are now accepted for transmission to Mrs. A. A. to % div. and 1 % bonus for 1903 \$140 b, Hongkong & China Gas Co., Ld & 30.25 Rauchboly, Mr. Doran, J. C. Batavia and the Dutch East Indies "direct." Hongkong Rope Manufactur-Tokio 30.22 Reid, T. H. Dymock, R.A., A. Scale of charges as follows :-\$10 for 1903 \$6421 ing Co., Ld..... Kochi Sawer, Mrs. W. E. Ferrier, Col. & Mrs. 1 lb. to 3 lbs.-\$1.15 Nagasaki Geo. Fenwick & Co., Ld. 15 per cent=\$3.75 for 1903 च । 🔎 । 💳 Foote, R.M., Capt. and Sinclair, A. Ships Passed The Canal. 4 lbs. to 7 lbs.-\$1.50 Interim of \$12 making \$16 for 1903 ... \$230 Kagoshima... Hongkong Ice Co., Ld. NE 6 -Smith, A. Findley 8 lbs. to \$1 lbs.-\$2.00 Mrs. Onlyard-4th March-Artemisia, Copack, Hongkong High-Level Tram-Oshima ,,,,, 30.00 Smith, C. W. French, Major G. A. Benlawers, Enandale, Cragoswald. 8th March Naha \$20 for year ending 31.11.1903 \$500 Greatest length 2 feet. ways Co., Ld. \$ 100 Grant, R.N., Eng. Lieut. Spalckhaver, W. O. C. -Heinrich, Formesa, Jeseric, Prins Jason, Ishigakijima. length and Girth 4 feet. \$13 for year ending 31.7.1903 \$124 b. Dairy Farm Co., Ld..... Steles, Mr. and Mrs. Sikh. 12th March - Achilles, Louis Tradit, Taihoku s a.m. 30.13 -No Insurance. Div. of \$3 for 1903...... \$37 ex div. Camphell, Moore & Co., Ld., 5 Hamilson, Major Steavenson, D. Marquit Bacquehem, Arabia. 16th March- Taichu...... Bell's Asbestos Eastern Sutherland, Mr. and VISITORS AT THE HOTELS. Hardy, R.N., Comman-Standard, Armenia, St. Meridian, Kelvin, Tainan 30,08 Mrs. J. and child der and Mrs. Maria, Kilda, 18th March-Voronej Breig. Koshun United Asbestos Qriental 11 30.03 NE THOMAS! Uffel, W. von Hewitt, F. T. B. Auel. 22nd March-Hyson, Oldenburg, Ness, Pescadores ... 90 cents for year ending 31.5.03 ... \$ \$9\$ b. \$210 b. .. \$30.08 NE 6 -Agency, Ld. 5 Watkins, R.E., Capt. Polmer, Chas, B. Holborow, Mr. Dornock, Orange, Palerma, Thyra. 25th March Weibalwai ... 9 Founders. 5 Ross, Miss V. and Mrs. Hubbe, P. -Stranburg, Arra gon, Palma, Salveig, 30th Guttlaff Hongkong Steam Water-boat Schurir, Otto Jeffues, H. W. Watson, Mr. and Mrs. March-Aragonia, Benmohr, C. Ferd Laciss, Sharp Peak ... Final of 6 % making 12 % for year ... \$153 % Co., Ld. Lee, Mr. & Mrs. J. B. Sirbund, A. S. Wenborn, S. T. Angus, Simoon. Interim of 59 cents per share for 1903 592 China Light & Power Co., Ld. \$ White, Dr. and Mrs. Whiley, Mr. Lowe, D. R. Francis, G. William Powell, Ld. Homeward -- 4th March - Hudson. 12th Swatow ,,,,, 9 a.m. -Lucas, Dr. H. F. Young, L. C. Guilders | Quarterly dividend of Tis, 10 paid} Tis, 285 sa. March-Annam. 16th March-Glenshiel. 18th Canton...... Maatschappij tot Mijn , Bosch-Yates, Mr. & Mrs C. C. MacSwayed, Mr. Lutgens, R. en Landbouw exploitatic in Mier, F. Martin, R. Macduff, Radnorshire, Seydlity. Langkat, Limited Victoria Peak Robin, G. Kernan, L. E. Shanghai & Hongkong Dyeing Arrivale at Home-4th March-Ponglace, Gap Rock ... ENR 4 CRAIGIRBURN. Mendoza, Mr. and Mrs. William, S. 30.14 64 - ENE 2 0 and Cleaning Co., Ld. Glenroy, Pyrrhus. 7th March-Jopan. 8th Macao Austen, R.M., Staff Helms, W. South China Morning Post, Ld. \$ First year \$45 March-Glenfarg, Georgian Prince, 12th Haiphong ... Smith, Mr. and Mrs. Surgeon and Mrs. March-Ernest Simons, Sachsen. 16th March Manila...... 29.96 90 45 KOWLOON. Telegraphic Address-"Righto." Benjamin, Kelly & Potts, Crafton, R. H. Grant Mitchell, Mr. Brooks, Mr. Ambria, Himero, Prinsesse Marie : 8th | Bacolod p.m. - -Smith, Mr. E. Grant Dann, G. H. Share Brokers. Telephone No. 148, P. O. Box No. 144, Murchie, Mr. 29.93 82 - NE 3 Clark, Lieut. T. F. March-Lowther Castle, Syria, Vindobona, Iloilo Woodward, Mr. & Mrs. Schiffman, Dr. R. Gaskeil, Mr. and Mrs. and children Iremonger, Lieut. Col. Yokura, K. NOTE :- b. = buyers, s. = sellers, sa. = sales. Menocal, D. A. pighshire, Glenlochy, Wursourg, MacEmen & Co. HERIOT BREWERY AGENTS IN THE FAR EAST FOR JOHN JEFFREY

Prickel

EDINBURGH,

FINE OCTOBER ALES.

A stock of JEFFREY's fine OCTOBER BREW has now been landed and is being offered in FIRKINS of 9 gallons;

KILDERKINS of 18 gallons;

OFFICE:

HOGSHEADS of 54 gallons;

PINTS (7 doz. to the case).

NOTICE.

COLD AFTER YOU PASS PORT SAID ON YOUR WAY HOME AND COLDER STILL SHOULD YOU GO VIA U. S. A OR CANADA. YET YOU CANNOT AFFORD TO LOSE THE SPLENDOUR OF THE MEDITERRANEAN OR PACIFIC SEA BY NIGHT. THEREFORE A TRAVELLING RUG BECOMES A NECESSITY AND WM. POWELL, LTD., ARE PREPARED TO SUPPLY FINE SCOTCH RUGS FROM \$8.50 TO \$25.00. THESE WILL BE FOUND IN THEIR GENTLEMEN'S DEPARTMENT AT 28, QUEEEN'S ROAD, WHERE THEY ALSO STOCK ALL OTHER TRAVELLING REQUISITES.

THE WAY, HAVE YOU HEARD THEY HAVE JUST RECEIVED

19 CASES OF NEW SPRING GOODS WHICH ARE NOW OPENED OUT, AND

OTHER LARGER SHIPMENTS FOLLOW TO STOCK THEIR NEW PREMISES

IN DES VŒUX ROAD WHICH WILL BE OPENED SHORTLY? ONE OF THE

CHIEF FEATURES OF THE BUSINESS WILL BE THE GRAND WINDOW DIS
PLAY WHICH CANNOT FAIL TO ATTRACT THE ATTENTION OF EVERY
ONE IN THE COLONY.

ROAD (UPSTAIRS).

· milling of the CVI is grathern.

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